

# City of Prescott

## City Council - Study Session



March 10, 2026 | 1:00 PM  
201 N Montezuma Street  
City Council Chambers, 1st Floor  
Prescott, AZ 86301

### AGENDA

The following Agenda will be considered by the **Prescott City Council** at its **Study Session** pursuant to the Prescott City Charter, Article II, Section 13. Notice of the meeting is given pursuant to Arizona Revised Statutes, Section 38-431.02. One or more members of the Council may be attending the meeting through the use of a technological device.

#### Viewing & Participation

This meeting may be viewed on Channel 64, Facebook Live or on the City's website: [City of Prescott Live Meeting Feed](#)

Public comments for Council may be submitted through the City website: [Public Comment Form](#)

1. **CALL TO ORDER**
2. **ROLL CALL**
3. **DISCUSSION**
  - A. Presentation & Discussion Regarding a Virtual Attendance Policy for the Members of City Council.
  - B. Presentation & Discussion Regarding the City Transfer Station Master Plan 90% Design Council Presentation.
4. **ADJOURNMENT**

Upon a public majority vote of a quorum of the City Council, the Council may hold an executive session, which will not be open to the public, regarding any item listed on the agenda but only for the following purposes:

- (1) Discussion or consideration of personnel matters (A.R.S. §38-431.03(A)(1));
- (2) Discussion or consideration of records exempt by law (A.R.S. §38-431.03(A)(2));
- (3) Discussion or consultation for legal advice with the city's attorneys (A.R.S. §38-431.03(A)(3));
- (4) Discussion or consultation with the city's attorneys regarding the city's position regarding contracts that are the subject of negotiations, in pending or contemplated litigation, or in settlement discussions conducted in order to avoid litigation (A.R.S. § 38-431.03(A)(4));
- (5) Discussion or consultation with designated representatives of the city to consider its

position and instruct its representatives regarding negotiations with employee organizations (A.R.S. §38-431.03(A)(5));

(6) Discussion, consultation or consideration for negotiations by the city or its designated representatives with members of a tribal council, or its designated representatives, of an Indian reservation located within or adjacent to the city (A.R.S. §38-431.03(A)(6));

(7) Discussion or consultation with designated representatives of the city to consider its position and instruct its representatives regarding negotiations for the purchase, sale or lease of real property (A.R.S. §38-431.03(A)(7)).

### **CERTIFICATION OF POSTING OF NOTICE**

The undersigned hereby certifies that a copy of the foregoing notice was duly posted at Prescott City Hall on 3/5/26 at 11:00 a.m. in accordance with the statement filed by the Prescott City Council with the City Clerk.

*Sarah M. Thornhill*

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Sarah M. Thornhill, City Clerk



TO: MAYOR AND CITY COUNCIL  
AGENDA: March 10 Study Session  
DATE: March 10, 2026  
DEPT: City Clerk  
ITEM #: 3.A  
SUBJECT: Presentation & Discussion Regarding a Virtual Attendance Policy for the Members of City Council.

## ITEM SUMMARY

This item is for Council and staff discussion regarding the possibility of offering a virtual/remote participation option for Council members to participate in Study Session and Voting Meetings if they are not able to be physically present.

## BACKGROUND

During the COVID-19 pandemic, the City of Prescott offered a virtual participation option temporarily for members of the City Council without a policy because of the state of emergency declared in the city and US overall. In early 2022, as the need for remote participation waned the city ceased offering this process. The Council does have an adopted attendance policy that allows for missed meetings and provides for the following (Council Rules of Procedure, Rule 7 Attendance): *"In accordance with the City Charter Article II, Section 11 if the Mayor or any member of the Council shall be absent from at least three consecutive called meetings without prior notice of the Council, City Clerk or City Manager, he or she shall by a majority vote of the balance of Council cease to hold office. Exceptions will be made in emergency situations."*

In recent months, staff has received several requests from Mayor Rusing to allow for remote attendance at City Council Meetings. The State of Arizona Open Meeting Law permits participation in a public body meeting IF a policy or procedure is adopted by the Council. Given the requests from the Mayor, City Clerk staff reached out to all Arizona jurisdictions for sample policies and procedures. Overall approximately 75% of other cities and towns do allow for remote participation, with some restrictions and stipulations. Based on this feedback, staff has put together the attached draft policy for Council's consideration on this matter (Attachment A). Based on Council feedback and discussion, if a majority of Council would like to have this option, a finalized policy will be brought back for Council approval and inclusion in the Council Rules of Procedure.

References have also been made to statutory updates related to ADA Access: Department of Justice, 28 CFR Part 35. This ruling applies to web based content accessibility for citizens and does not relate to remote participation by members of a body. Additionally, the city deadline to ensure these criteria are being appropriately met is April 2027 based on city population.

In addition to adoption of a Policy, there are financial considerations related to upgrading the Council Chambers sound and IT system in order to implement this policy in the most appropriate manner. Cost estimates for these updates and improvements would be approximately \$11,000 and would need to be added to the FY27 IT Budget to cover costs.

## FINANCIAL IMPACT

There is no fiscal impact associated with this item at this time, however, there will be future financial impacts for equipment upgrades and staff time should Council determine they want to move forward with implementation.

## **RECOMMENDED ACTION**

This item is for discussion only. Pending discussion, Council should provide direction to staff regarding bringing forward a virtual attendance policy for future adoption and inclusion in the Council Rules of Procedure.

## **ATTACHMENTS**

1. DRAFT Council Virtual Participation Policy

## Council Virtual Participation Policy

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This policy is intended to govern the City's standards related to remote/virtual participation for members of the City Council in Council Meetings. The State of Arizona Open Meeting Law permits participation in a public body meeting if a policy or procedure is adopted by the Council. This policy shall apply only to members of the Council for Study Sessions or Voting Meetings, and is not applicable to Board, Commission or Committee Members.

Council members should endeavor to be physically present at all council meetings, in order to adequately participate. However, in the event of a medical issue, illness or family situation, and with prior approval of the presiding officer, no more than two (2) Council members may participate in a Study Session or Voting Meeting via Teams.

1. **Definitions:** Remote Participation shall mean participation of any member of the City Council remotely via a Teams Meeting established by the City IT Department, during a Study Session or Voting Meeting in Council Chambers at Prescott City Hall. Remote Participation is not available for meetings conducted outside of the Council Chambers
2. **Executive Session:** Remote Participation is not permitted for Executive Sessions
3. **Notification:** Council members must notify the City Clerk, or designee, not less than two (2) business days in advance of the meeting that they will need to have Remote Participation access to the meeting; if less than two (2) days notification is provided, Remote Participation will not be available
4. **Remote Participation of the Mayor:** In the event that the Mayor needs Remote Participation access to the meeting, the Mayor Pro Tem will serve as the presiding officer for the meeting
5. **Participation:** Members of the City Council utilizing Remote Participation will view the meeting via Teams on a city issued device, and shall not be logged into their CivicClerk Board Portal
  - a. When the remotely participating member wishes to ask a question or comment following staff presentation and Council discussion, they shall

raise their hand in Teams and the City Clerk will notify the Presiding Officer

- b. When participating remotely, Council members must vote verbally on items
6. **Limitations:** No member of the City Council may Remotely Participate for more than two (2) consecutive meetings and no more than six (6) meetings in any calendar year.



TO: MAYOR AND CITY COUNCIL  
AGENDA: March 10 Study Session  
DATE: March 10, 2026  
DEPT: Public Works  
ITEM #: 3.B  
SUBJECT: Presentation & Discussion Regarding the City Transfer Station Master Plan 90% Design Council Presentation.

## ITEM SUMMARY

This item is to provide a 90% project update for City Contract No. 2025-143 with J.R. Miller & Associates, Inc. (JRMA) for the Transfer Station Master Plan. The location of the project is shown on the attached site map, Exhibit A.

## BACKGROUND

The City of Prescott Transfer Station is located at 2800 Sundog Ranch Road. The facility was built in 1991 due to the closure of the Sundog Landfill. Over the past 33 years, some improvements have been made to accommodate increasing customers and tonnage. Currently, the Transfer Station is open 6 days a week (Monday–Saturday). The current operations are at or are nearing maximum capacity of customers and tonnage. A recent separate Capital Improvement Project, the new fee booth and scales project, was completed to alleviate congestion and help the queuing distance for inbound and outbound customers. The master plan will review and evaluate the current Transfer Station operations and facility and recommend changes to increase capacity, improve operational efficiency and safety, and mitigate concerns from neighboring properties.

The master plan goals include recommendations for optimal site use and traffic flow patterns for different customers through the use of pavement markings, striping, and signage, separating municipal solid waste trucks from self-haul customers and residential and commercial vehicles, site and size recommendations for a new transfer station building with one pull-through load-out tunnel to handle future solid waste volumes, recommendations to retrofit the existing transfer station building for recycling, provide better and more efficient operations for the dumping, processing, and chipping of green waste which is currently handled multiple times, increase the area for inert debris storage for bi-annual crushing operations, consolidate, increase, and improve parking lots for employees, collection vehicles, and commercial dumpsters and residential containers, consolidate wash racks into one (1) area that meets ADEQ requirements, recommendations to mitigate operational impacts from lighting, dust, noise, odors, and traffic especially on surrounding properties, CCTV and card access building and site security improvements, fencing and landscaping improvements for site safety and to provide visual/operational buffers from adjacent properties, and storm water drainage improvements.

J.R. Miller & Associates, Inc. (JRMA) was contracted in May 2025 to lead a collaborative master planning effort. Since then, JRMA has led a kick-off coordination meeting in February 2025 and progress meetings in July, August, October, and November 2025 to review master plan development. JRMA delivered a 90% draft of the master plan, including narrative and maps, to the city in January 2026.

## FINANCIAL IMPACT

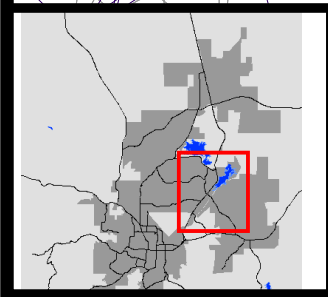
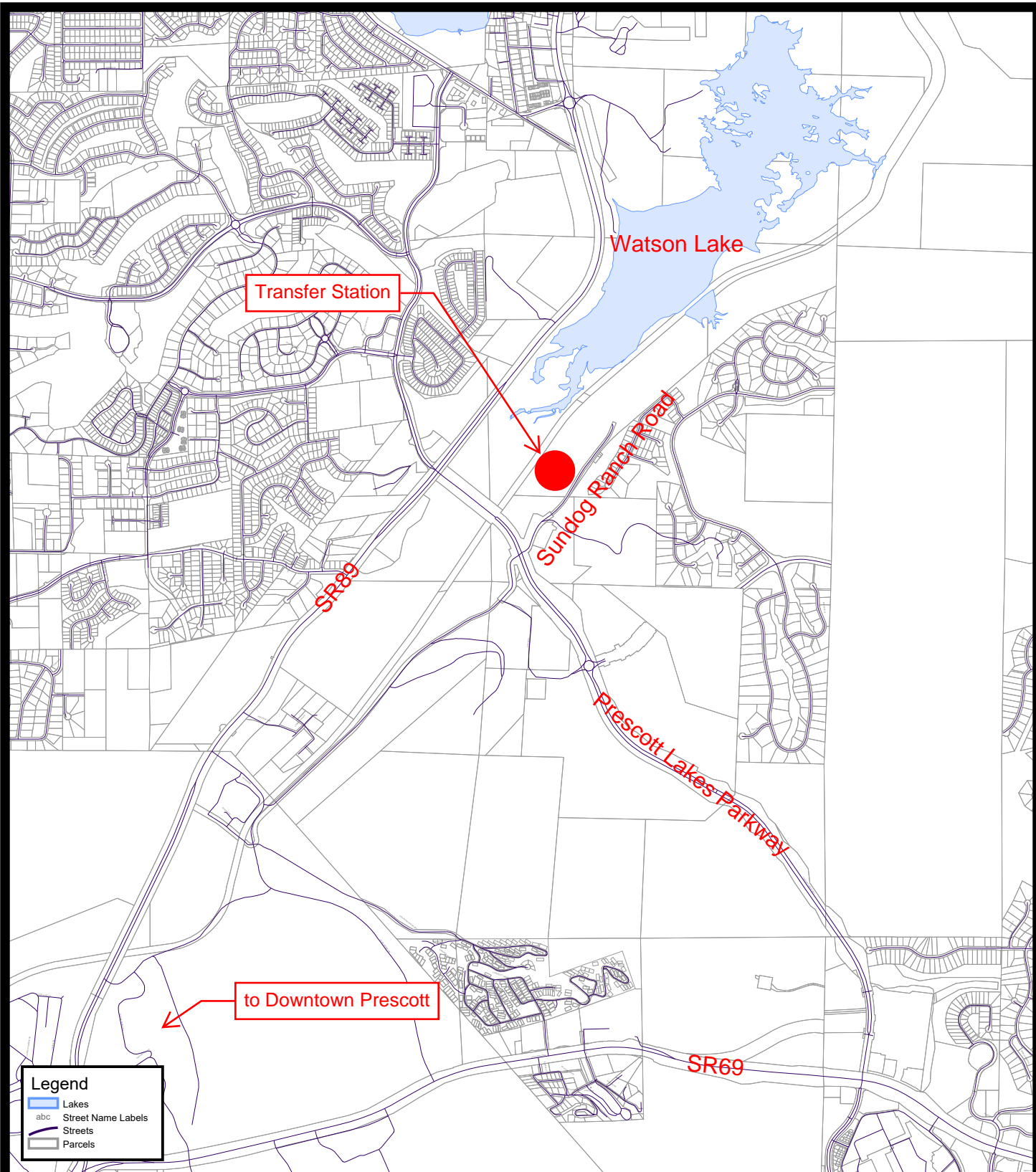
There is no financial impact at this time.

## **RECOMMENDED ACTION**

This item is for discussion only. No formal action will be taken.

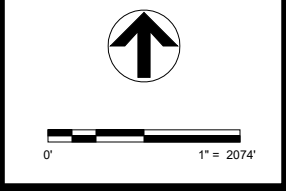
## **ATTACHMENTS**

1. Exhibit A - Location Map DRAFT
2. Transfer Station Master Plan 90% Design Narrative
3. Transfer Station Master Plan 90% Design Presentation



# Transfer Station

This map is a product of  
The City of Prescott





## Programming Narrative

### City of Prescott Transfer Station Master Plan

Issue Date: February 23, 2026  
Prepared For: City of Prescott  
Prepared By: JRMA & Kimley Horn



# Master Plan Programming Narrative

Project No: 6209  
Client Name: City of Prescott, AZ  
Document Title: TS Master Plan Programming Narrative  
Document Status: Draft For City Review  
JRMA Project Manager: Jim Yost, AIA

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## Document History and Status

Revision	Date	Description	Author	Reviewed	Approved
01	1/30/26	Draft for City Review	Brett Wolfe	Jim Yost	
02	2/13/26	90% Final Draft	Brett Wolfe	Jim Yost	
03	2/23/26	City Council Presentation	Brett Wolfe	Jim Yost	



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## **Appendices**

**Appendix A. Transfer Station Schematic Site Plan Overall Layout (Figure 3-1)**

**Appendix B. New Transfer Station Schematic Floor Plan (Figure 3-2a)**

**Appendix C. Transfer Station North and East Elevations (Figure 3-2b)**

**Appendix D. Transfer Station South and West Elevations (Figure 3-2c)**

**Appendix E. Site Topographic Survey (Figure 3-3a)**

**Appendix F. Conceptual Site Grading Plan (Figure 3-3b)**

**Appendix G. Conceptual Site Drainage Plan (Figure 3-3c)**

**Appendix H. Preliminary Drainage Rational Method**

**Appendix I. Schematic Site Plan - Recycling Route (Figure 3-4a)**

**Appendix J. Schematic Site Plan – Transfer Truck Route (Figure 3-4b)**

**Appendix K. Schematic Site Plan – Commercial MSW Route (Figure 3-4c)**

**Appendix L. Schematic Site Plan – Self Haul Route (Figure 3-4d)**

**Appendix M. Site Utility Plan (Figure 4-1-4)**

**Appendix N. Landscape Exhibit (Figure 4-1-11)**

# 1. Introduction

## 1.1. Purpose

The City of Prescott (City) Transfer Station Master Plan Narrative (MPN) summarizes key background information, City provided data, and high-level design concepts evaluated by the design team during the Master Plan Project (Project). The intent of the MPN is to document and present basic design criteria, site requirements, discipline specific design elements, conceptual site layouts, and other key considerations for the development of an approximate 20-year master plan for the City’s transfer station and overall solid waste operations. Through the Project’s design process, the JRMA Team, which includes Kimley Horn, has worked in partnership with City staff to develop numerous conceptual layouts of the site that meet the operational requirements and long-term planning of the site. This master plan narrative is a compliment to the various site layouts, grading, drainage, vehicle routing, and building layouts contained in Sections 3, 4, and the appendices of this document. The following narrative contains additional design details for specific disciplines, key site focus areas, as well as a preliminary opinion of probable cost.

## 1.2. Project Background

The existing City of Prescott Transfer Station (TS) is located on an approximately 22-acre parcel located at 2800 Sundog Ranch Road, Prescott, Arizona. The existing TS was built in 1991 and has undergone a few modifications over the past 33 years to increase customer capacity and tonnage handled at the facility. The TS is open 6 days per week (Mon-Sat) and sees daily customer counts exceeding 350 tons per day (TPD) but typically handles between 250-300 TPD. The site is primarily utilized for residential self-haulers and City collection vehicles but does receive a few commercial haulers as well.

The Project was commissioned by the City to focus on on-site optimization, a new transfer station building, separation of City and self-haul (resident) traffic, expanded inert and green waste operations, both City and employee vehicle parking, and numerous other key improvements to enhance overall operations at the site. The MPN is focused within the site boundaries only, but the City recently finished an off-site CIP project to relieve congestion on Sundog (and the TS site) that includes new scales, a scalehouse, bypass lanes, and paving repair work.

The entire master planning project also evaluated other key off-site considerations to minimize operational impacts to the surrounding development to the east and the Peavine Trail along the south and west sides of the site. These considerations included the following:

- Visual and viewshed
- Noise and proximity to key operational activities
- New TS building orientation for both sound and visual impacts
- On-site traffic management (routing, queuing, and scaling) to further reduce traffic impacts to Sundog Ranch Road beyond current scale and scalehouse project.

### *Project Features and Goals*

The project features and goals as communicated to the JRMA Team by City staff in numerous in-person and virtual meetings as well as those stated in the November 2024 SOQ includes the following:

- Enhanced separation of City users from self-haul (public) users
- Expanded green waste and inert receiving and processing areas.
- Reduce operations and maintenance (O&M) costs to include reducing multiple handling of materials.
- Expanded green waste, municipal solid waste (MSW), and inert drop-off areas to accommodate more users and provide safer vehicle movements.
- New 17,000 sq ft Transfer Station
- New Employee parking as current area has paving and drainage issues.
- Provide for future flexibility of operations.
- Re-purpose existing TS into a recycling transfer operation.
- Close evaluation of site drainage and paving considerations.
- Commercial and transfer parking accommodations.
- Residential Bin and Roll-off storage areas.
- High-level utility evaluation.
- Archaeological and endangered species pre-determination
- Accommodate future projections of tonnage and traffic for expanded material types.
- Incorporate enhanced and current transfer station standards.
- Overall site masterplan layout to include traffic analysis, siting of all key operations, new TS siting, any new/modified connections to public right-of-way.

### 1.3. Background Documents and Information

Key documents used for the development of the master plan narrative include the following:

- 2005 Transfer Station Master Plan – Completed April 2005
- Annual Scale House Vehicle Data from City
- City of Prescott Request for Statement of Qualifications Transfer Station Master Plan, CIP 2408-001 – Dated November 2024
- City of Prescott Public Works developed site plan dated 11/4/24

## 2. Design Criteria

Tables 2-1, 2-2, and 2-3 present the master plan's waste-receiving volumes, traffic, and parking design criteria. The design criteria were received from the City during programming meetings and through email communication throughout the project's execution.

### 2.1. Waste Volumes

Table 2-1. Waste Volume Design Criteria

Facility Component	Parameter	Value
Total MSW Quantities	Average Tons per Year	80,000
	Average Tons per Day	250-300
	Peak Tons per Day	500 (weekends)
	Self-haul/City Commercial & Residential	45k/35k
Green Waste & Self Haul Wood Debris	Self-Haul Wood Debris Tons per Year	6,000
	Green Waste Tons per Year	10,000
Inert Collections	Storage Capacity in Tons	20,000 (w/ 25-50% growth)

### 2.2. Traffic Volumes

Table 2-2. Traffic Volume Design Criteria

Facility Component	Parameter	Value
Self-Haul MSW & Brush/Green Waste	Peak Brush Pile Vehicles	10-15
	Peak MSW Vehicles	15-20
	Peak Total Vehicles per Hour	25-30
City MSW (Commercial & Residential)	Average Vehicles per Day	15
	Peak Vehicles per Hour	5
Total City & Self-Haul Vehicle Count	Total Vehicles Per Year	100,000

### 2.3. Vehicle Parking

Table 2-3. Vehicle Parking Design Criteria

Parking Area	Vehicle Type	Value
City Commercial Parking	Side Loaders	15
	Rear Loads	4
	Commercial Front Loaders	5
	Roll Off Truck	1

Parking Area	Vehicle Type	Value
	Total City Parking Stalls	40
Transfer Hauler Contractor Parking	Trailers	11
	Tractors	5, plus 1 yard goat
Employee Parking	Total Employee Parking Stalls	40
	Additional Requirements	Provide Double Stalls for Trailers

## 2.4. Queuing and On-site Traffic

The city requests that all primary roadways be paved for durability, maintenance, and performance. The new scalehouse plaza will handle self-haul transactions, as the City desires that as much City traffic and queuing be handled on-site and off the public right-of-way. This required an additional scale for City vehicles and space for queuing both packer trucks and transfer trucks. Separate entrances/exits have been provided for City vehicles and transfer operations to avoid congestion at the north public entrance.

Substantial self-haul queuing has also been provided on the site to further minimize impacts to the public right-of-way. The primary goal of the queuing area is to keep vehicles flowing through the site without backing up and disrupting other site operations or traffic on Sundog Ranch Road.

The MSW and Brush unloading space and stalls have been increased significantly from the current site operations. This will allow better separation of the various self-haul user types, improve safety of vehicle maneuvering and unloading, as well as allow the City to manage peak customer counts which typically occur on the weekend.

The new transfer station and site layout allows for 15 vehicles unloaded simultaneously and the green waste area can accommodate approximately 12 vehicles at the same time.

## 3. Site and Facility Layouts

This section provides the various site, grading, drainage, utilities, and transfer station building layout drawings developed during the master plan Project. **All figures shown in this section are provided as enlarged images (printed as 11x17 drawings in hard copy version) in the appendices of this report.**

### 3.1. Site Master Plan Layout

The new transfer station building will be located on the existing City of Prescott Sundog Ranch Road site. The new TS building will occupy an area that currently serves as the inert debris operation area at the site. The new proposed site master plan layout is shown in Figure 3-1 (Appendix A) and includes the following primary features:

- New 17,000 sq ft TS
- Relocated Green Waste operations to reduce multiple handling of material

- New City vehicle parking area
- New employee parking
- Relocated inert debris operations
- Significantly more on-site queuing of self-haul vehicles
- New City/Commercial unmanned scale
- Relocated storage areas for residential bins and roll-off containers
- Landscaping at site perimeter for screening

The site layout shown below was developed by the design team and City staff through numerous options, and potential layouts before selecting the layout shown below.

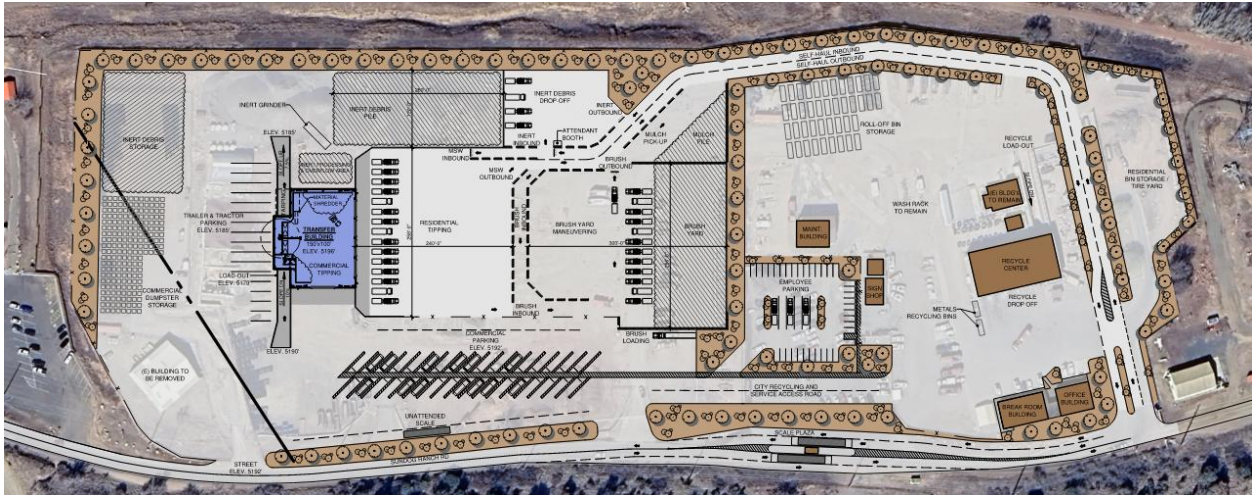


Figure 3-1. Transfer Station Schematic Site Master Plan Overall Layout

### 3.2. Transfer Station Building

The new transfer station floor plan is shown in Figure 3-2a (Appendix B). The floor plan shows a conceptual layout, to include four (4) primary openings on the north self-haul side of the building and one enlarged door on the east side for City/Commercial vehicles. Pushwalls have been shown on the west side and along the load-out port to facilitate handling and storage of MSW material. Two restrooms and a small office have been provided within the building to reduce the need for TS staff to travel up to the administration building during the day. The floor plan and transfer station drawings will be developed in more detail at future stages of design under a separate contract. In addition to the floor plan, conceptual building elevations are shown in Figures 3-2b (Appendix C) and 3-2c (Appendix D) to demonstrate representative building architectural design from all four sides of the facility.

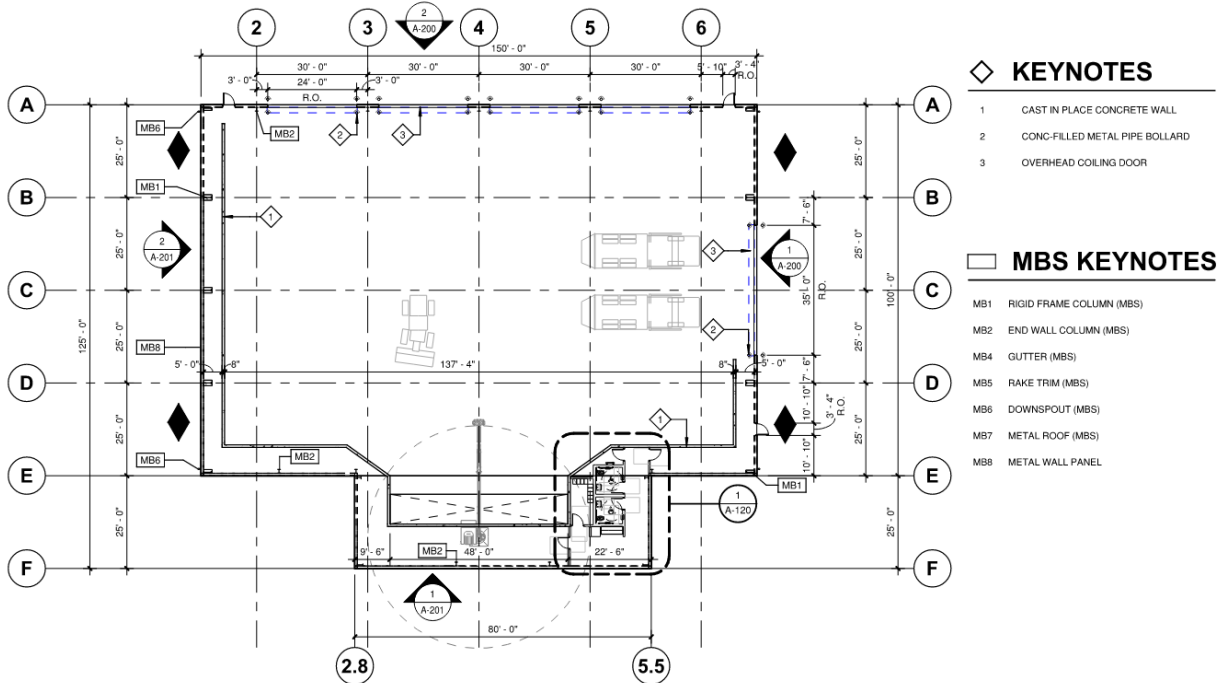


Figure 3-2a. New Transfer Station Floor Plan

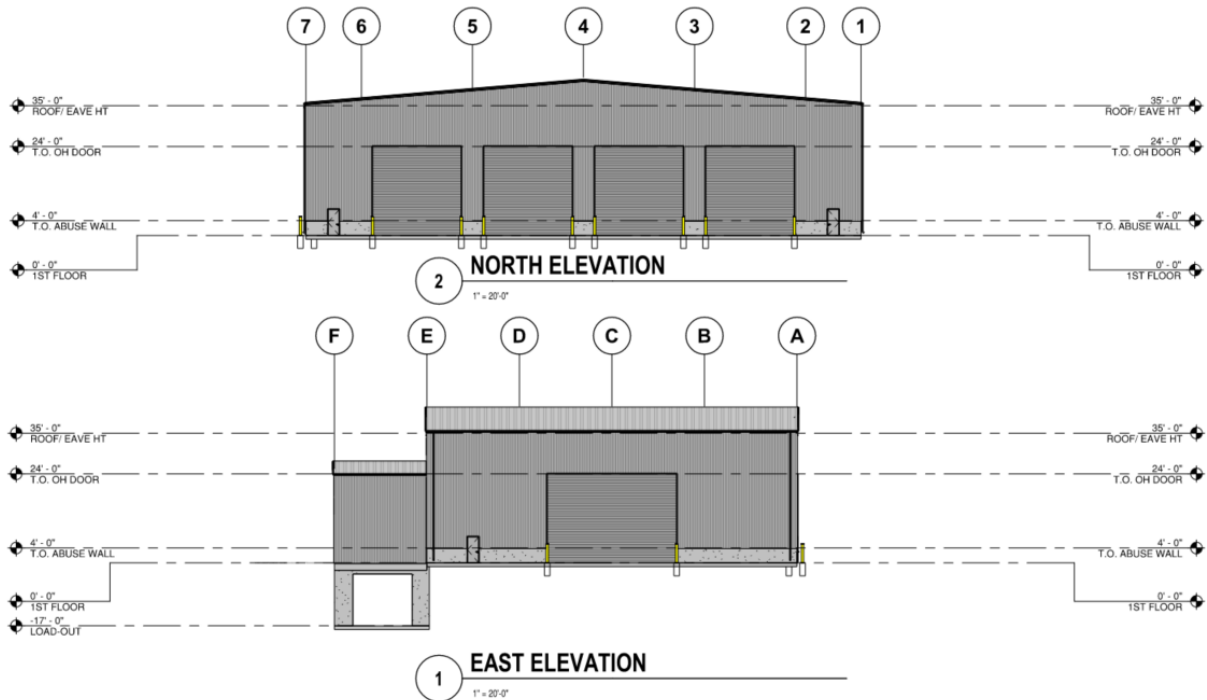


Figure 3-2b. Transfer Station North and East Elevations.

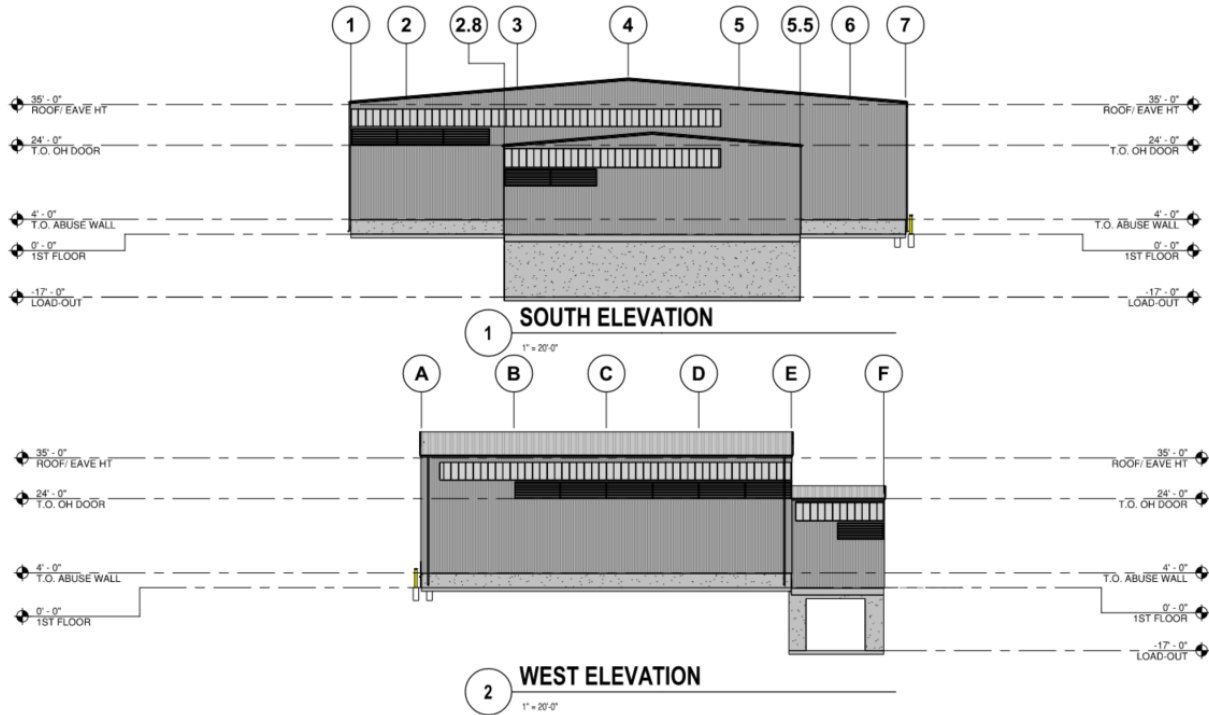


Figure 3-2c. Transfer Station South and West Elevations

### 3.3. Site Topography, Grading and Drainage

The site generally slopes from South to Northwest. An east-west bio swale runs through the middle of the existing site, conveying stormwater runoff to the west from approximately two-thirds of the existing facility.

Existing inlets exist at several locations along the west side of the property. Onsite runoff outfalls offsite to the west ultimately through a swale adjacent to Peavine Trail.

Topographic Survey was completed in July 2025 with the sealed sheets included below in Figure 3-3a (Appendix E).

The civil design considerations are as follows:

- Paved areas will have a minimum of 0.50 percent and a maximum of 10 percent slope, depending on vehicle type and limitations.
- Unpaved areas will have a minimum of 2 percent and a maximum of 33-percent slope, depending on the area's projected use.
- Grading, drainage, and access will also incorporate the necessary permanent and temporary BMPs to control onsite sediment and erosion.
- Future detailed grading design and specification standards will follow City Standard Details and Standard Specifications for public works projects along with City General Engineering Standards.

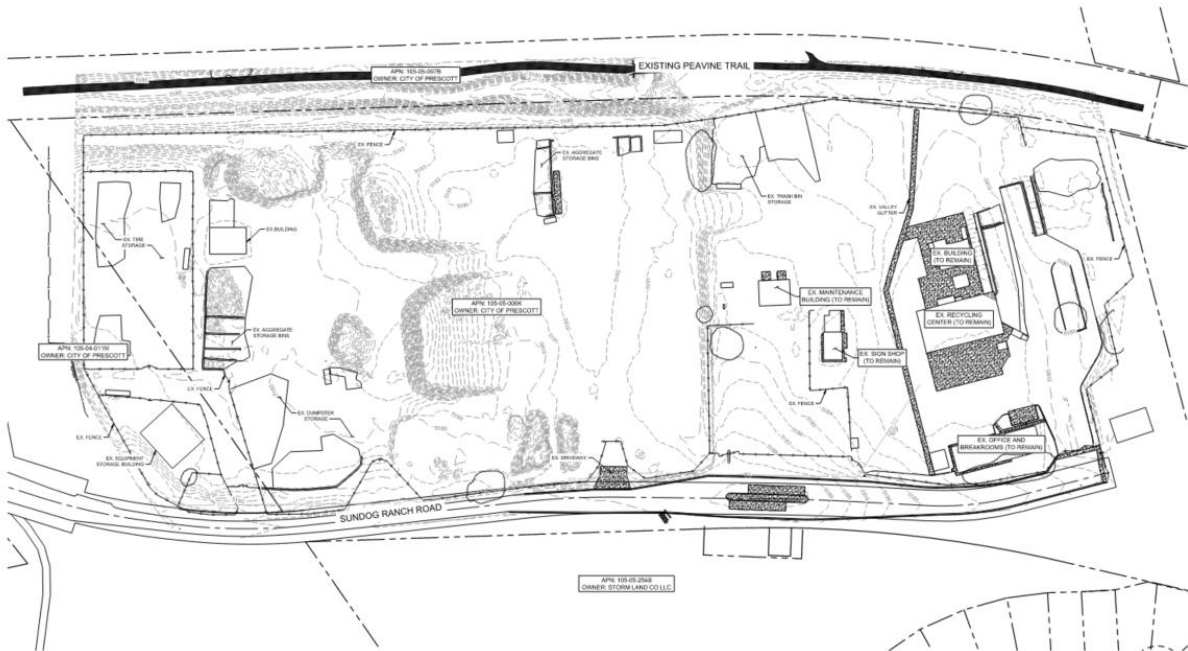


Figure 3-3a. Site Topographic Survey

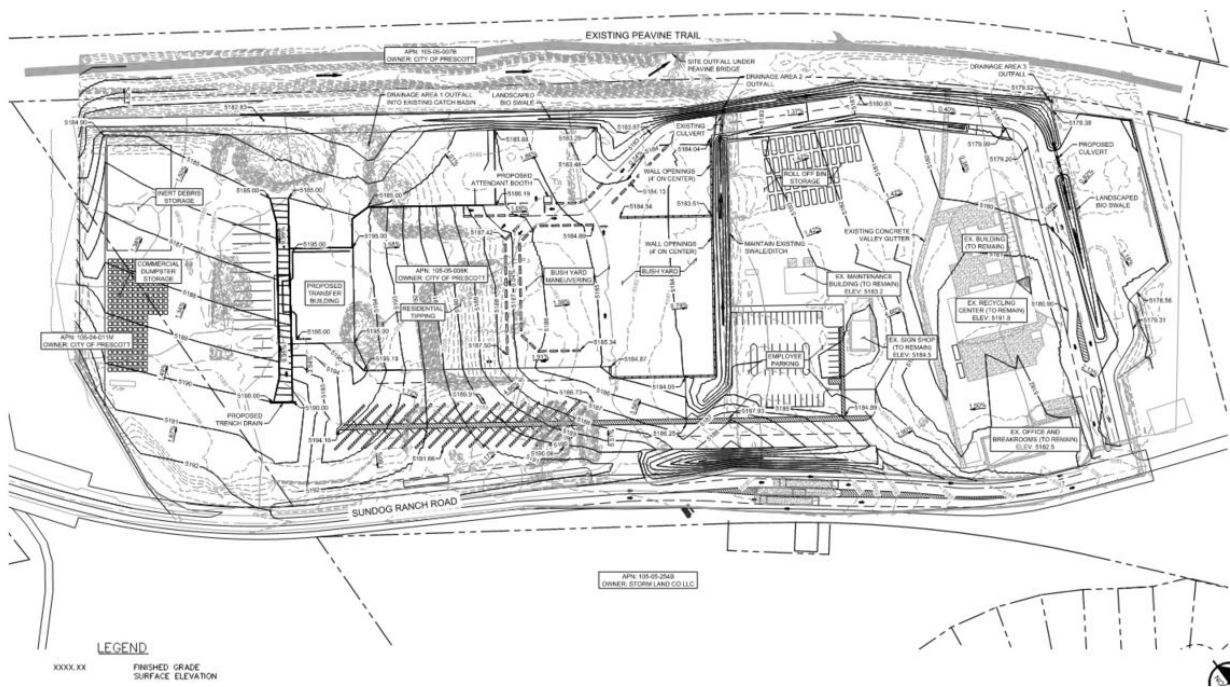


Figure 3-3b. Conceptual Site Grading

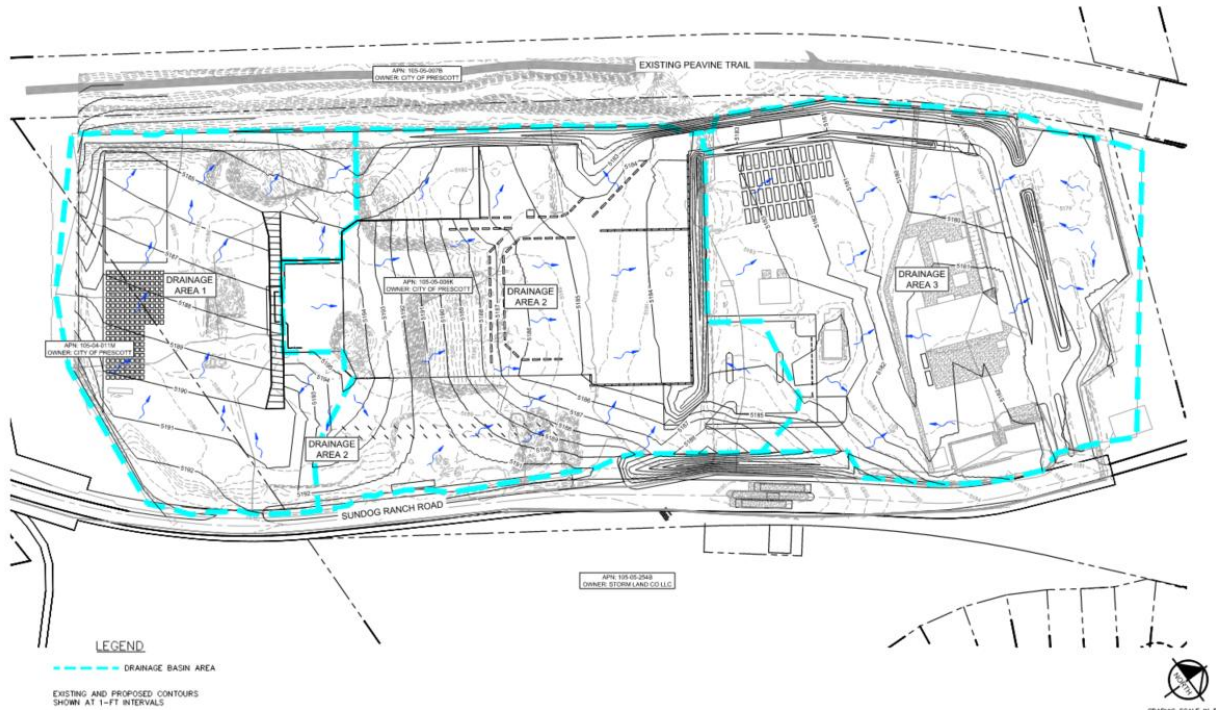


Figure 3-3c. Conceptual Site Drainage

### 3.4. Site Traffic Drawings

The following drawings show the various vehicle types and routing on the site. A primary goal of the City's TS Masterplan was to separate City traffic from residential (self-haul) traffic as much as possible on the site. To accommodate this goal, separate entrances to the site were provided for City/Hauler and self-haul vehicles. The new City/Hauler site entrances are also served by a new automated scale to provide hauling tickets and to accurately measure the total tonnage of material received at the facility.

Figure 3-4a (Appendix I) shows the recycling collection and transfer trucks (in green) as they route on and off Sundog Ranch Road onto the site and deliver or haul away recyclable materials from the existing TS, which will be upgraded to manage recyclable materials in lieu of MSW. The entering City recycling collection vehicles will use the new scale to obtain a tonnage of recycling materials received and the exiting transfer trucks will obtain a weigh ticket for over-the-road travel to the recycling facility.

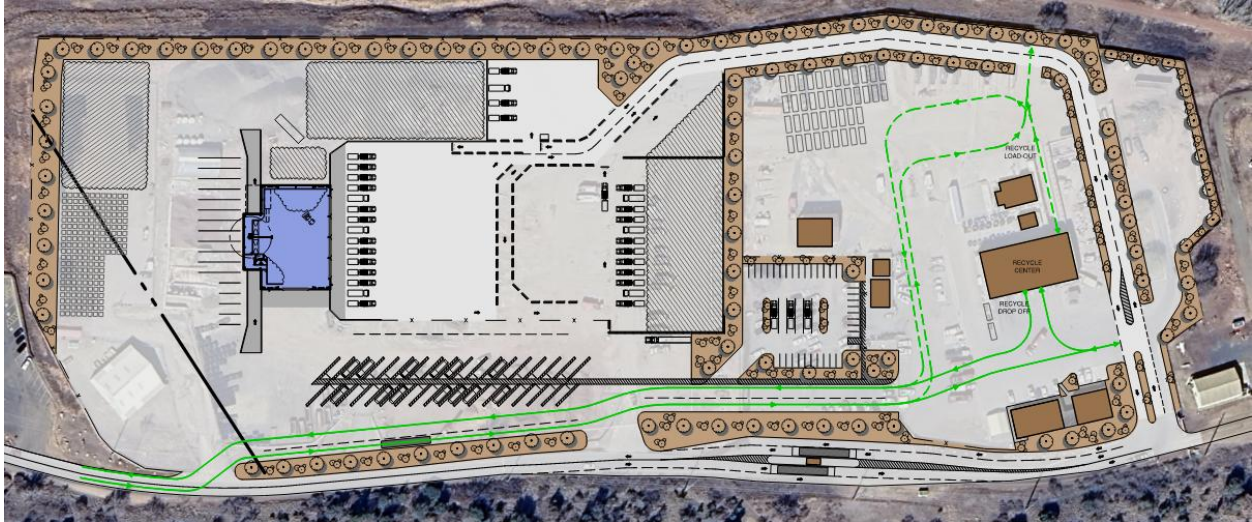


Figure 3-4a. Traffic Flow On-site for Recycling Collections

Figure 3-4b (Appendix J) demonstrates the routing of MSW transfer vehicles around the site. Key functions include yard goat connection to trailers, loading of trailer within the load-out tunnel, tarping of the trailer, and on-site scaling to obtain a weigh ticket for over-the-road travel to the landfill. The City contracts their transfer operations with an outside vendor, and the site was designed to provide this contractor with a dedicated location on the site and minimal crossover traffic of other City/Merchant vehicles.

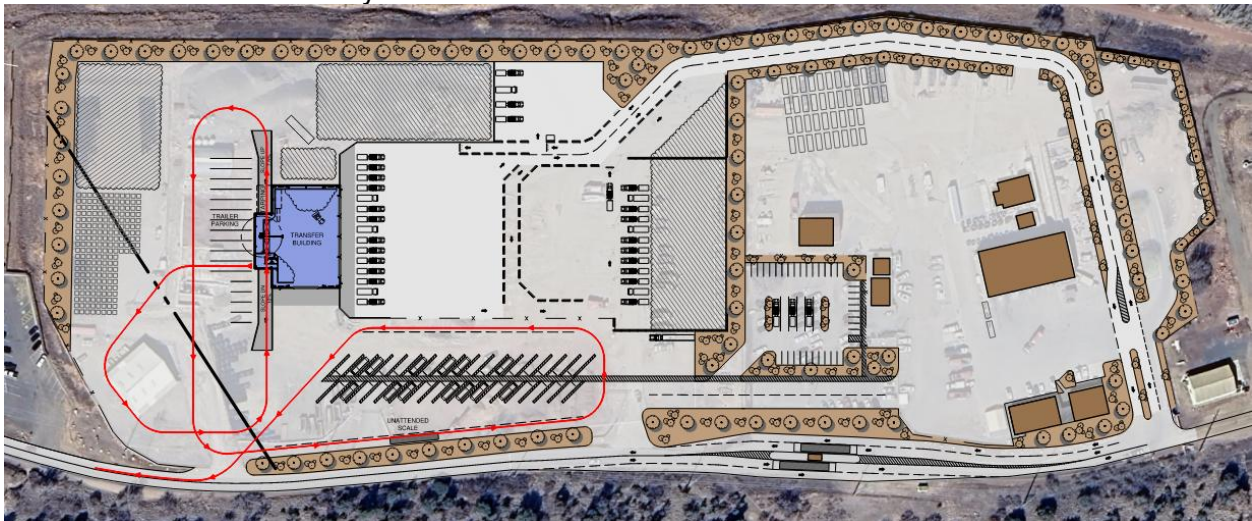


Figure 3-4b. Traffic Flow On-site for Transfer Vehicles

Figure 3-4c (Appendix K) shows the typical routing of MSW collection vehicles on the site. All City vehicles will go across the on-site scale when they enter the site to confirm tonnage of MSW to the facility. They will then unload within the new TS and either directly exit the site to go back out on another collection route or park in the designated City vehicle parking area on-site at the end of the day. Any merchant collection vehicles that utilize the transfer station will get both an inbound and outbound scale weight to be appropriately charged by the City at their current disposal tip rate.

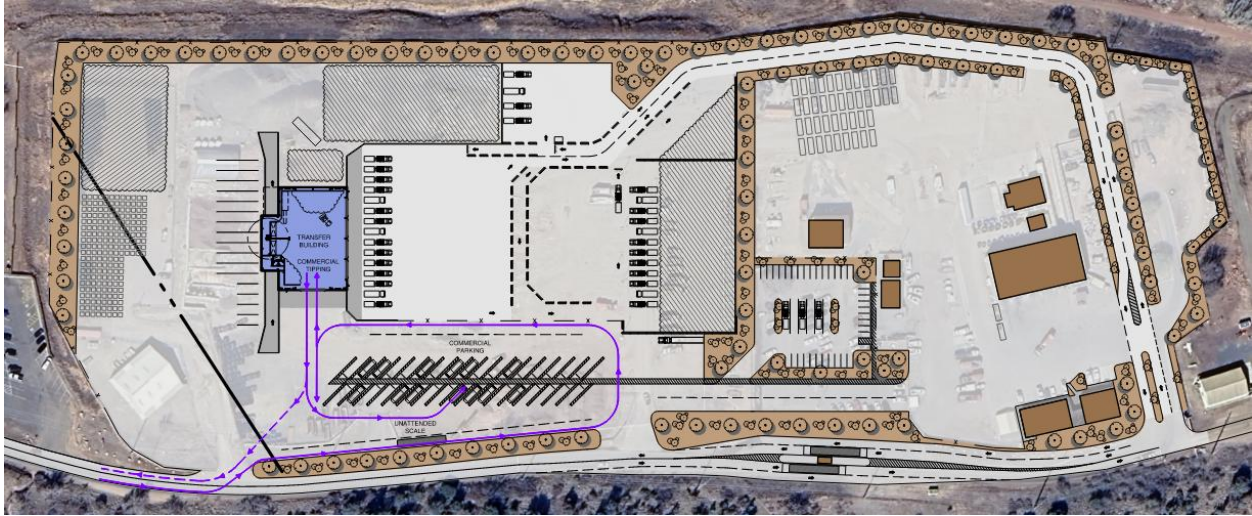


Figure 3-4c. Traffic Flow On-site for Commercial MSW

Figure 3-4d (Appendix L) shows the public self-haul vehicle routing at the new scale plaza on Sun Dog Road as well as routing to the various drop-off areas on the site. The primary drop-off areas on site include the following:

- MSW at new TS
- Green waste at Brush Yard
- Inerts at the designated drop-off area
- HHW at the existing TS
- Metal drop-off into roll-off near existing TS

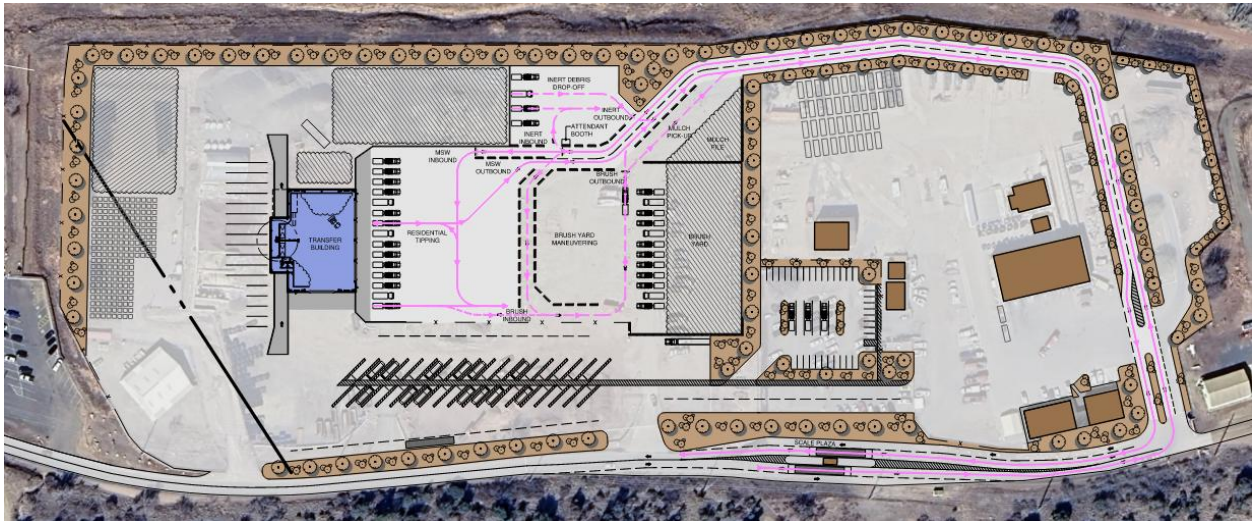


Figure 3-4d. Traffic Flow On-site for Self Haul

## 4. Discipline Specific Project Development

This section identifies discipline specific project criteria applicable to the overall site and facility development. The disciplines included are as follows: site/civil, architectural, structural, mechanical, electrical, and environmental. Preliminary design criteria, to be used for future detailed design phases, are provided for the buildings, site development, and other key elements of the Project.

### 4.1. Site Civil

#### 4.1.1 General

General site/civil development criteria for Prescott TS master plan include:

- Minimize interference with the ongoing operations through phased construction.
- Provide a landscape buffer and bioswale along the west and north perimeter of the existing facility.
- Minimize or prevent impacts to adjacent property owners and businesses and major view corridors.
- Surface water controls in accordance with City and County Standards.
- Provide new utility service connections for water, sanitary sewer, storm sewer, electricity, telephone, and cable in compliance with requirements from the respective utility.
- Site access and roadways will be designed for self-haul vehicles with trailers, commercial packer trucks, roll-off trucks, and transfer vehicles, as applicable to the various parts of the site.
- The site will be fenced with gated entrance and exit points.
- Concrete paved roadways will be located in areas used by transfer vehicles and commercial haulers due to their long-term durability. Asphalt pavement will be used for areas of the site that experience lighter-duty traffic including the roadways for self-haul customers.

#### 4.1.2 Roadway Geometry

The site roadway layout is designed based on the following criteria identified in coordination with PSWD:

- Turning radius for self-haul vehicles with trailer – 24 feet
- Turning radius for MSW collection (packer) truck – 42 feet
- Turning radius for transfer vehicle – 45 feet
- Maximum grade for regularly used roadways – 6%
- Loadout tunnel maximum grade – 10%
- Back-in distance in front of parking and loadout ports – 150 feet
- Adequate length is needed for all vehicles to straighten out before and after the scales, and to enter and exit all buildings

#### 4.1.3 Paving

##### 4.1.3.1 Roadway Pavement

Site Paving shall be per the Geotechnical Report.

#### 4.1.4 Site – Utilities

- Utilities exist along Sundog Ranch Road and the interior of the existing facility. This includes City Water, Sewer, Storm, Gas, Electric, Fiber optic and Cable. See Utility Exhibit below in *Figure 4-1-4* (Appendix M).
- Points of connection for the building fire sprinkler and domestic water systems, including revision to on-site fire hydrant locations, fire department connections, backflow prevention, double detector check valves and landscape irrigation meters will be from the existing City Water Main located in Sundog Ranch Road.
- Site fire flows will be achieved using existing and new fire hydrant locations and connections.
- New off-site water service connections will include all water appurtenances including backflow preventers, check valves, domestic and irrigation water meters necessary for the on-site water system. The off-site water connections will be coordinated with the City water department and the Fire Marshall.

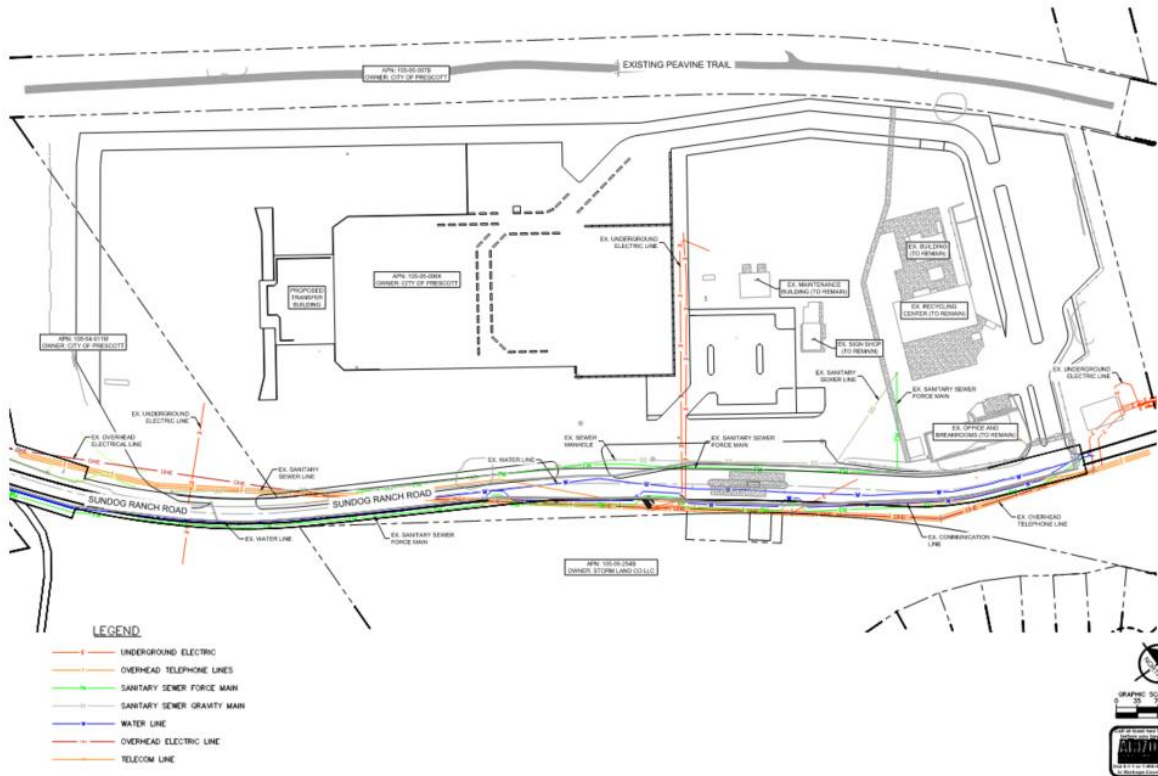


Figure 4-1-4. Site Utilities

#### 4.1.5 Storm Water Management Systems

The City of Prescott General Engineering Standards addresses Stormwater Management requirements for development. This includes but is not limited to pre vs post development detention requirements, onsite conveyance and best management practices. As the project site is within 0.50 miles of Granite Creek, a notice of intent to discharge (NOI) will be required from Yavapai County Environmental Services Department (YCESD).

Overall, the surface water management system includes:

- Conveyance facilities, including pipes, ditches, and perimeter swales.
- Impacted (non-storm) water management including floor drains/collection trenches, curb and gutter, piping, treatment (i.e., OWS) that will be discharged to the City sanitary sewer in conformance with City code.

#### 4.1.6 Temporary Erosion and Sediment Control

Throughout construction, temporary drainage and erosion control facilities and source controls will be provided. Erosion and sediment control measures will likely include:

- Providing temporary cover over exposed soils and stockpiles.
- Using silt fencing between construction activities and downstream water courses.
- Installing check dams along existing and temporary ditches.
- Directing stormwater runoff to a temporary sediment trap or portable treatment tanks for treatment prior to discharge to the downstream system.
- Installing stabilized construction entrances to prevent sediment from discharging off-site.
- Installing permanent cover measures as soon as possible after construction is complete.
- Providing water quality source controls during construction to prevent pollutants from coming into contact with stormwater which would include safe handling of petroleum products including proper storage and maintenance of vehicles and equipment.
- Requiring construction traffic to utilize the tire wash prior to entering City streets

#### 4.1.7 Collection System

Stormwater on impervious areas of the site will drain to a collection system comprised of curb, gutter, catch basins, ditches, and trench drains.

Management of stormwater in areas with higher potential for pollution such as the hot load area will be designed to be collected and routed to the sanitary sewer where necessary.

#### 4.1.8 Detention System and Outfall

Stormwater will ultimately route through the three designated subbasins (DA 1, DA 2 and D3 shown on the Drainage Exhibit in Appendix X) to existing outfall points. Stormwater will then drain into landscaped bio swales prior to the ultimate outfall under the Peavine Trail.

#### 4.1.9 Hot Load

A designated hot load area, defined as “a load that is smoking, smoldering, or emitting flames,” will be provided for both City trucks and self-haul truck operations. During normal operations, the hot load area will drain to the stormwater system. An exterior hose reel will be located on the northeast corner of the TS which will provide potable water for dousing hot loads. The drainage of the hot load areas will allow for containment prior to discharging to storm or manual cleanout. The detailed design of these areas will occur during the next phase of design.

#### 4.1.10 Site – Retaining Walls

Because of the site topography and to support required grade separation of material loadouts there will be two primary retaining walls required for the new Transfer Station building. One retaining wall will be required for the loadout level to create sufficient grade separation for transfer trucks. The second retaining wall will be required for the inerts pile for both unloading and processing. This second wall will help to keep self-haul residents separate from City operations.

#### 4.1.11 Landscape Architecture

The landscape design for the Prescott Transfer Station will be provided adjacent to new buildings and where paved areas are permanently removed, to preserve and enhance the natural environment with an additional focus on visual screening, most notably along the Peavine Trail. See the Landscape Exhibit below in *Figure 4-1-11* (Appendix N).

- Planting within the property lines will be in accordance with The City of Prescott Land Development Code, General Development Standards.
- Planting will be used to minimize visual impact to adjacent uses and to major view corridors to surrounding areas.
- Planting adjacent to site access and roadways will follow guidelines for sight distance triangles and pedestrian visibility.
- The plant palette will utilize low-water use, drought tolerant plants from the ADWR Prescott Active Management Area.
- A minimum of one tree per 25 linear feet of required landscaped area and four shrubs per 250 sq ft of required landscaped area shall be provided.
- Any combination of grasses, ground covers, and inorganic materials may be used for the balance of the required landscaping; a dressing of gravel, decomposed granite, or mulch shall be implemented in landscaped areas.
- City of Prescott Land Development Code requires parking lots with more than 50 spaces to provide landscaped parking islands and a perimeter parking lot buffer planting. To provide shade, visual interest, and enhanced appearance of parking lots and adjacent areas, landscaped parking islands will be provided.

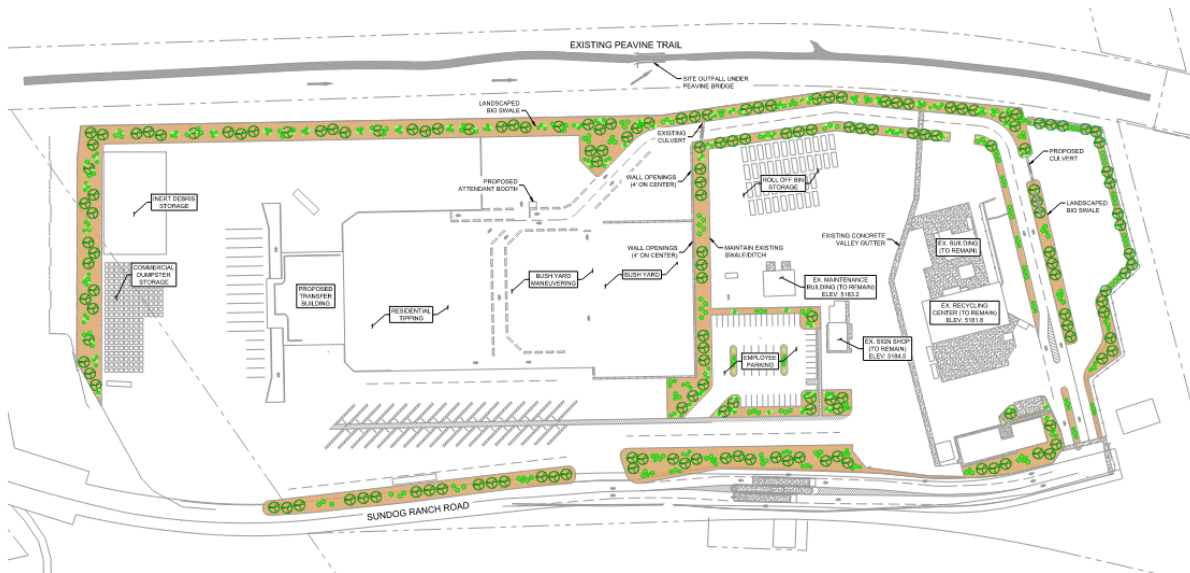


Figure 4-1-11. Landscape Exhibit

#### 4.1.12 Preliminary Plant Recommendations

(from Arizona Department of Water Resources – Prescott Active Management Area: Low Water Use & Drought Tolerant Plant List)

- Achillea millefolium – Yarrow
- Amelanchier utahensis – Utah Serviceberry

Ceanothus greggii – Mojave Buckbrush  
Chamaebatiaria millefolium – Fernbush  
Cupressus arizonica – Arizona Cypress  
Salvia greggii – Autumn Sage  
Teucrium chamaedrys – Wall Germander  
Quercus gambelii – Gambel Oak

#### 4.1.13 Irrigation System

A permanent, underground water irrigation system including an EPA WaterSense approved programmable timer will be installed. The irrigation system design will ensure long-term health and growth of the landscape, taking into consideration the water-demand characteristics of plant or landscape materials used.

#### 4.1.14 Fencing

- The entire perimeter of the project site will be secured with security features comprised of a combination of retaining or block walls and fencing. Note – the site perimeter fencing does not follow the property lines but does encompass the entire facility for security purposes.
- A six-foot-tall chain link fence, to match existing, or potentially a block wall, on the south and west property lines, is proposed around the site perimeter. The need for a block wall will be evaluated during the detailed design phase.

## 4.2. Architectural

- The overall architectural objective is to suggest contextually sensitive and visually attractive structures.
- The transfer station location on site and architectural features will take into account view corridors from adjoining properties.
- Design drawings will be based on utilizing a steel framed structure. Columns will be located adjacent to exterior walls to provide clear space for tipping and traffic areas.
- The main structural framing system will be a steel frame structure with special braced frames.
- Primary steel will be primed and epoxy painted.
- The TS building will have a low slope roof which slopes from east to west.
- Exterior finishes for the buildings will consist of metal and translucent panels with concrete at lower wall areas as protection measures from damage from vehicle and/or operations.
- Building components will be selected for recycled material content where feasible.
- Consideration will be given to voids in wall and ceiling spaces to prevent harboring of vectors.
- Metal wall and roof panels will have factory applied custom color, Kynar 500 paint finish.
- Vehicle access down to the loadout ports will be provided for efficient loading of transfer station trucks.

### 4.3. Structural Engineering

Preliminary structural engineering requirements for the City of Prescott TS include the following:

- 10" structural reinforced heavy-duty floor in operational areas of transfer station.
- Coordination with the geotechnical engineer to select foundation types appropriate for metal building system (MBS) of the transfer station building.
- Exterior building walls shall be cast in place concrete for lower-level abuse protection with a combination of metal stud and structural steel elements for exterior wall construction.
- Incorporate code required loads including wind, seismic, dead, and live.
- A low slope roof framing layout.
- Site retaining walls: Mechanically stabilized earthen (MSE) berm, CIP, and soldier pile wall plans, profiles, and details as needed.
- All primary steel members will be primed and painted.
- Metal wall and roof panels will have factory applied custom color, Kynar 500 paint finish.
- Adequate spans will be designed such that no columns will be required on the tipping floor.

### 4.4. Mechanical Engineering

#### 4.4.1 Plumbing and Fire Protection

Site piping, building plumbing and fire protection systems will include the following design elements:

- Cold water systems will be shown on mechanical drawings inside buildings and to within 5 feet of buildings; any other hydrants will be shown on the civil, utility drawings.
- Hot water systems will be shown on mechanical drawings within the buildings.
- Wastewater flows that may contain grit, grease or oil will be diverted through a grit trap and an oil/water separator (OWS) structure before leaving the site as sanitary sewage.
- Fully automatic wet and dry-pipe fire sprinkler systems will be designed in conformance with National Fire Protection Association, Inc. (NFPA) 13.
- The selected fire protection vendor will submit sealed sprinkler drawings to the City Fire Department for approval.
- Plumbing fixtures will be designed to adhere to accessibility requirements per the Accessibility Code.

#### 4.4.2 Heating, Ventilation, and Air Conditioning

- Mechanical systems will be designed in accordance with the 2018 International Mechanical Code as amended by the City of Prescott.
- Ventilation systems will be designed in accordance with ASHRAE 62.1-2018 International Energy Conservation Code.
- Mechanical systems will conform to the 2018 International Energy Conservation Code and will exceed prescribed efficiencies for HVAC equipment.
- Mechanical systems will be integrated into a building automation system (BAS).
- Where roof access is not available, is impractical, or detracts from the aesthetic appearance of the building, equipment will be designed to be accessed and maintained from the ground level.
- Heating, cooling and ventilation are provided for the Administration Building.

## 4.5. Electrical Engineering

- Electrical demand requirements will be established for the proposed facility.
- Design will include the primary power systems for the buildings, process equipment, interior and exterior lighting, general and HVAC power.
- Performance specifications will be developed for the fire alarm system, as will device layout drawings.
- Interior and exterior lighting systems will be designed for the proposed buildings.
- Energy efficient lighting systems will be implemented as appropriate.
- Light pole locations will be selected to be light pollution reduction compliant.
- Screening systems may be considered for exterior lighting features to reduce or eliminate off-site light pollution.
- Telecommunication and IT demand requirements will be provided by the City. Wall jacks, raceways, and related wiring will be provided to meet the criteria.
- Security system requirements will be provided by the City. Raceways and related wiring will be provided to meet the criteria.
- Building personnel access doors will be equipped with card access system.

## 4.6. Environmental

The project is anticipated to include local funding only. If federal funds are acquired, National Environmental Policy Act (NEPA) and supporting documents would be required.

### 4.6.1 Clean Water Act (Section 404/401)

No potential waters of the U.S. (WOTUS) are located within the Study Area; therefore, Section 404/401 permitting will not be required.

### 4.6.2 Wetland and Riparian Areas

The U.S. Fish and Wildlife Service (USFWS) National Wetland Inventory (NWI) database identified a freshwater pond located within the Study Area; however, based on review of historic aerials, no freshwater ponds, wetlands, or riparian areas are present within the Study Area.<sup>1</sup> Freshwater emergent wetland and riverine areas are located adjacent to the Study Area in the Watson Woods Riparian Preserve.

### 4.6.3 AZPDES Stormwater Permit

If construction will disturb more than one acre of land, a Section 402 Arizona Pollutant Discharge Elimination System (AZPDES) permit and a Stormwater Pollution Prevention Plan (SWPPP) will be required from the Arizona Department of Environmental Quality (ADEQ).

### 4.6.4 Groundwater

Information from the Arizona Department of Water Resources (ADWR) website was reviewed for water wells located within one mile of the site. According to well registration records, groundwater exists at greater than 80 feet below ground surface.<sup>2</sup> However, groundwater levels can fluctuate due to seasonal variations, groundwater withdrawal or injection, and other factors.

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<sup>1</sup> U.S. Fish & Wildlife Service, National Wetlands Inventory (2026). <https://www.fws.gov/program/national-wetlands-inventory/wetlands-mapper>

<sup>2</sup> Arizona Department of Water Resources (ADWR). Arizona Groundwater Site Inventory (GWSI). Accessed: January 26, 2026. <https://azwatermaps.azwater.gov/gwsi>

#### 4.6.5 Cultural Resources

Based upon a preliminary review of data from AZSITE, a portion along the western boundary of the Study Area and the southeast and northeast corners of the Study Area have been previously surveyed; however, the majority of the Study Area has not been previously surveyed. A portion of the mapped boundary of one previously recorded cultural resource consisting of a portion of the historic Santa Fe, Prescott, and Phoenix (SFP&P) Railway Line intersects with the northwest corner of the Study Area. The SFP&P line was originally built in 1892-1893 and abandoned in 1983. However, aerial imagery shows the footprint of the abandoned rail line that runs adjacent to the western border of the Study Area and does not intersect with the Study Area. Historic topographic maps and aerial imagery show that the Study Area was disturbed by construction of a wastewater treatment facility between 1966-1972 and redeveloped into a wildfire training academy in the mid-2000s. It is possible the Study Area may have once contained archaeological sites; however, the overall archaeological and cultural sensitivity of the Study Area is low due to the previous disturbances. A Class I records review would need to be completed to document any cultural inventories and/or site that may occur. Upon conclusions of a Class I records review, it may be determined that a Class III survey may be required for any areas not previously surveyed or where surveys are over 20 years old. However, due to the highly disturbed nature of the Study Area, a Class III survey recommendation is unlikely.

#### 4.6.6 Biological Resources

Kimley-Horn reviewed the potential for protected species for this area using tools from the USFWS and Arizona Game and Fish Department (AGFD). Based upon the USFWS Information for Planning and Consultation (IPaC) (consultation code 2026-0038356, on January 20, 2026) list, there are 11 threatened, endangered, or candidate species that should be evaluated for the project area. These species include the Mexican wolf (*Canis lupus baileyi*), Mexican spotted owl (*Strix occidentalis lucida*), southwestern willow flycatcher (*Empidonax traillii extimus*), yellow-billed cuckoo (*Coccyzus americanus*), Gila chub (*Gila intermedia*), Gila topminnow (*Poeciliopsis occidentalis*), Gila trout (*Oncorhynchus gilae*), loach minnow (*Tiaroga cobitis*), spikedace (*Meda fulgida*), monarch butterfly (*Danaus plexippus*), and suckley's cuckoo bumble bee (*Bombus suckleyi*). There is no federally designated critical habitat within the project vicinity. The AGFD on-line environmental review tool (OERT) was accessed January 20, 2026 (Project ID: HGIS-27199) and listed the Arizona toad (*Anaxyrus microscaphus*), golden eagle (*Aquila chrysaetos*), yellow-billed cuckoo, monarch, southwestern willow flycatcher, and bald eagle (*Haliaeetus leucocephalus*) as occurring within three miles of the project vicinity.

According to AGFD's habitat suitability model, 71% of the habitat adjacent to the Study Area is potentially suitable for the golden eagle; 23% of the habitat adjacent to the Study Area is potentially suitable for the yellow-billed cuckoo; 25% of the habitat adjacent to the Study Area is potentially suitable for the southwestern willow flycatcher; and 100% of the habitat adjacent to the Study Area is potentially suitable for the bald eagle. The Study Area is highly disturbed; however, based on its proximity to Watson Woods Riparian Preserve, noise impacts to listed species from construction should be analyzed in a Biological Evaluation.

#### 4.6.7 Floodplain Encroachment

The project is located on Federal Emergency Management Agency Flood Insurance Rate Map (FEMA FIRM) Panels 04025C2058H, which has an effective date of 03/06/2018. The Study Area

is within the mapped Flood Zone X.<sup>3</sup> Impacts to the floodplains are not anticipated but should be confirmed during final design.

#### 4.6.8 Hazardous Materials

Based on ADEQ eMaps, the Sundog Transfer Station does not appear to have violations; however, a Preliminary Initial Site Assessment (PISA) would be recommended during final design to investigate the potential for hazardous material concerns.

Under the 1970 Clean Air Act, the U.S. Environmental Protection Agency (EPA) established the Asbestos National Emission Standard for Hazardous Air Pollutants (NESHAP). Since the transfer station is a publicly owned facility, any demolitions or renovations onsite would require inspection for asbestos containing material (ACM) by an Asbestos Hazard Emergency Response Act (AHERA) certified Building Inspector.

### 4.7. Sustainability

The City of Prescott TS will incorporate sustainable measures in the design and operation of the facility, but the City is not pursuing LEED certification.

## 5. Transfer Station

The new City of Prescott TS is a rectangular building with the commercial tipping area on the east side, self-haul tipping area on the north-central portion, MSW loadout on the south, and material shredding on the west side. The new TS building (including the tipping floor, lower level, and shredding areas) will encompass a total of approximately 17,000 sq ft of floor space and processing areas to service all commercial and self-haul customers visiting the site.

### 5.1. Architectural

- The proposed building will be designed with a minimum interior clear height of 30 feet.
- The door for commercial tipping will be located and sized to allow for packer and roll-off trucks with minimum 35 feet clear width and 24 feet clear height on the door.
- Self-haul egress and ingress doors will be 24 feet clear width and 20 feet clear height on the doors
- Push walls will be provided in the tipping area. Wall heights, materials, and cladding will be suitable for use in specific areas.
- Due to the nature of the facility use, the proposed building design will be provided with specific detailing features necessary to minimize maintenance requirements.

#### 5.1.1 Exterior Building Materials

The exterior materials will be consistent with colors and materials throughout the project.

- Vehicle access doors will be overhead steel coiling doors.
- Roof panels will be steel standing seam with a white Kynar finish for heat island effect reduction.

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<sup>3</sup> Esri, HERE, Garmin, FAO, NOAA, USG: Hazards FEMA Maps (2026). <https://hazards-fema.maps.arcgis.com/apps/webappviewer/index.html>

- Curb-mounted, acrylic, dome skylights will be provided on the roof to increase natural lighting.
- Downspout locations will be designed to convey rainwater away from vehicle and pedestrian traffic.
- Concrete walls and painted concrete filled steel bollards will protect the building in key areas.
- Translucent wall panels will be included around the perimeter of the facility to provide natural lighting into the tipping floor areas.
- Condensate will be controlled inside the building by providing a layer of foam insulation between the structural metal deck and the metal roofing.

#### 5.1.2 Interior Materials

- Floor finishes will be flat, unsealed concrete.
- Interior finishes shall be selected to maximize interior light levels and reduce glare.
- Interior partition materials at the top load areas only will be vertical metal liner panel from top of concrete push walls to underside of roof structure above.
- Interior window and door frame materials will be painted and composed of hollow metal.

#### 5.1.3 Interior Specialties

- Interior signage will meet applicable code requirements, including those at the restrooms, locker rooms and exits.
- Interior columns will be protected from vehicular door openings with yellow plastic covered, concrete-filled steel bollards.
- Fire extinguishers will be provided per fire department requirements.

## 5.2. Structural

- The TS will be a fully enclosed, unheated, structural steel building with metal siding and metal roofing.
- Clear span construction with no interior columns across the tipping floor of the TS.
- The commercial and self-haul tipping areas will both have a minimum of 30 feet inside clearance from the bottom of the trusses to the lowest hanging features, such as lighting, sprinklers, etc. The minimum structural clearance of the deepest structural frame sections will be at the east and west walls.
- The lower level will be CIP concrete construction with a clear height of 15 feet. Areas/opportunities for bird perching will be as minimal as possible.
- The tipping floor will be a flat floor design with dry floor cleanup of the floor on a daily basis. Therefore, no trench drains or wash down systems are provided for the facility.
- Push walls will be provided around loadout openings and along west wall of transfer station for additional MSW storage/pile.
- The tipping floor will be constructed with high strength reinforced concrete.
- Wear indicators will be provided on the tipping floor.
- A new tamping crane will be located at the new top load opening. The crane will be mounted on an elevated concrete pedestal approximately 42-inches high. A concrete column (2 feet by 5 feet) will transfer the crane load down to the lower-level foundation.
- Hydrostatic pressure will be mitigated by use of a water proofing material and pipe drainage system located around the lower-level walls.

- The foundation system is preliminarily assumed to consist of shallow concrete spread and continuous footings.

### 5.3. Mechanical

- Outdoor design conditions:
  - Summer: 82 degrees Fahrenheit (F) dry bulb, 64 degrees F wet bulb
  - Winter: 26 degrees F
- It is assumed only the office/restrooms will be conditioned. The following equipment is proposed:
  - (1) – 36 MBH split system heat pump.
  - Manufacturer: Trane-Mitsubishi
  - Model Number (outdoor / indoor): PUZ-AK36NL / PEAD-AA36NL
  - Electrical: 208/60/1, 34A MCA, 35A MOCP
- It is assumed that the transfer station will be mechanically ventilated at a rate of 6 air changes per hour. The following ventilation equipment is proposed:
  - (6) – 7,500 CFM roof mounted exhaust fans (1.5 HP each)
  - Manufacturer: Greenheck
  - Model: GB-300
  - Electrical: 208/60/1, 13.8A MCA, 20A MOCP
- Indoor air design conditions will not vary from outdoor design conditions (no heating or cooling provided).
- Carbon monoxide and nitrogen dioxide monitoring on the tipping floors and in the loading bays to provide for increased ventilation when warranted.
- The TS will be ventilated as follows:
  - Mechanical exhaust ventilation systems will be used.
  - Natural cross ventilation will be provided through sidewall louvers and doors at the walls of the building, determined to be most suitable due to prevailing winds. Actual air movement will be measured, and mechanical exhaust will be reduced as natural ventilation meets more of the ventilation requirements.
  - Mechanical ventilation will be provided by upblast exhaust fans located strategically on the roof. The fans will draw air evenly across the tipping floor through louvers on the walls and will provide additional airflow in the vicinity of the hoppers. The fans will run continuously during operations to provide the minimum ventilation rate of 6 air changes per hour. A manual override will be provided.
  - A separate mechanical ventilation system will be provided for the lower-level loadout area to remove vehicle emissions. The system is sized for 1.5 cubic feet per minute per sq ft and will be regulated by diesel exhaust levels.
- If the City wants to add dust collection in the future, hoods can be installed over the compactor hoppers and ducted to a cyclone dust collector that could be located outside the TS.

## 5.4. Plumbing and Fire Protection

- The estimated plumbing fixture types, quantities, and fixture unit calculation is provided below:

<b>PLUMBING FIXTURE UNIT CALCULATION</b>							
MARK	DESCRIPTION	QTY	FIXTURE UNITS		TOTAL FIXTURE UNITS		NOTES
			SFU	DFU	SFU	DFU	
WC-1	WATER CLOSET, FLUSH TANK	2	5	4	10	8	1
L-1	LAVATORY	2	2	1	4	2	
MS-1	MOP SINK	1	3	2	3	2	
SK-1	OFFICE SINK, SINGLE BOWL	1	2	2	2	2	
HB-1	HOSE BIBB	4	2.5	-	10	-	
FD-1	FLOOR DRAIN, 2"	2	-	2	-	4	
TOTAL		12			29	18	
			TOTAL DFU	=	18	DFU	
			TOTAL SFU	=	29	SFU	
			CORRESPONDING GPM	=	23	GPM	
NOTES:							
1. CALCULATIONS PER 2018 IPC, BASED ON PUBLIC FLUSH TANK TYPE FIXTURES.							

- The required plumbing utility connections for the new transfer station are:
  - (1) – 4” sanitary sewer line.
  - (1) – 1-1/2” domestic cold water service line.
- The proposed water heater for the new transfer station is:
  - (1) – 30-gallon electric tank type water heater, 3kW input.
- The building will be provided with a fully automatic fire sprinkler system in conformance with NFPA 13.
- A dust suppression misting system is not currently planned for the facility, but further evaluation of the need will be provided during detailed design phase.
- A few hose bibbs will be provided around the facility for hot load and rare wet clean-up. The tip floor will typically be cleaned using a dry cleanup approach.
- Non-potable cold-water lines will be electrically heat-traced to prevent freezing.
- A dry pipe fire protection system will be utilized in the TS to mitigate the potential of frozen pipes. The system will be designed in the construction period in accordance with design guidelines (performance specification) issued in the construction drawings.

## 5.5. Electrical

### 5.5.1 Distribution

- Electrical service to the TS will be established by Arizona Public Service.
- Power will be supplied by a pad-mount utility transformer.
- All underground conduits and cables shall be provided as part of the project to the main service entrance switchboard.
- Primary distribution equipment and conductors will be provided by the utility company.
- The utility company will provide metering equipment and current transformers.
- The utility company and general contractor’s responsibility will be defined in the contract documents.

- Power distribution parameters shall include the following: complete lighting and power distribution for all building areas and equipment.
- Site Lighting Shall conform to criteria listed below, per area type:

Table 5-5. Site Lighting Criteria

Area:	Minimum	Average	Avg/Min	Max/Min
Parking Lot	0.2		4:1	
Inspection/Gate Area		10	3:1	
Vehicle Loading		5		5:1
Roadway (local)		0.5	6:1	

(Light Levels measured in foot candles (fc))

- Light trespass shall conform to section 3-12-8 LIGHT TRESS PASS within CHAPTER 3-12: OUTDOOR LIGHTING REQUIREMENTS of the City of Prescott general code.
- Utility load shall be calculated by an estimation of watts per square foot, per area type (load estimation is subject to change given further design):
  - 1) Building:
    - 35 W/sq ft X 15,000 sq ft = 525,000 W
  - 2) Site:
    - 0.025 W/sq ft (Proposed concrete paving & proposed AC paving)  
X 332,823 sq ft = ~6700 W
  - 3) HVAC:
    - 10,000 W
  - 4) Total = ~541,700 W

## 5.6. Stationary Equipment

A new tamping crane will be provided for the new transfer station to improve load quality and tonnage within the trailer.

The shredder currently being used for green waste will be re-used for the new operations and layout.

A new MSW shredder will be provided by the City and is excluded from the opinion of probable cost.

The existing transfer station's tamping crane will be re-used within the building to accommodate recyclables transfer.

Roll-off containers and ecology blocks for bunkers, green waste operation, and other necessary storage areas will be provided by the City.

## 6. Administration Building

The existing administration building will remain as is and was not evaluated as part of this master planning project.

## 7. Maintenance Building/Area

The existing general maintenance building will be maintained for light-duty repairs.

The existing vehicle maintenance building modifications are planned for the site.

## 8. Relocation of Transfer Station and Waste Operations

The design team provided a high-level cost evaluation for relocating the City's solid waste operations from the Sundog Ranch Road location to an alternate location within the city. A key challenge with relocating the overall waste operations is that several existing buildings and site features would need to be reconstructed at the new site, resulting in a much higher project cost for the relocated facility. The following facilities and infrastructure would need to be developed at a new TS site:

- Administrative building
- Recycling drop-off/transfer
- Primary site utilities
- Vehicle maintenance
- Self-haul scales and scalehouse
- Light maintenance building
- Wash Rack
- Full site grading, drainage, and landscaping
- Fencing and security
- Site lighting

As a result of these additional features, the opinion of probable cost for the development of a new transfer station and supporting facilities at a new site is estimated to be approximately \$62M, which is more than three times the cost of upgrading the existing site. See Section 9 for full cost details of upgrading the Sundog Ranch Road TS facility.

While the cost of a new site would be much greater than upgrading the Sundog Ranch location, a new site would provide fully updated facilities for City staff and would also avoid phased construction which will be required to maintain operations at the existing TS site.

In addition to the cost savings, retaining the existing Sundog Ranch Road TS site offers the advantage of reduced environmental and planning efforts for a new site. An additional item to consider is if the new site is not centrally located the City may experience an increase in illegal dumping.

Further detailed evaluation of a new site would be required once a property is selected and was not included within the master plan project scope.

## 9. Cost Estimate

The following section (Table 9-1) provides an opinion of probable construction costs (OPCC) based on the Master Plan conceptual drawings developed for the Sundog Ranch Road TS. Due to the preliminary phase of design concepts for both the site and new TS, this OPCC should be considered equivalent to a Class 5 AACE Estimate. As the design progresses (future detailed design contract) these estimated costs will be revisited and updated. In addition, if the City has existing contractor relationships, these drawings and future detailed design packages could be provided to the contractor to establish a more refined and accurate cost estimate for the City's use.

Table 9-1. OPCC Summary

**Opinion of Probable Cost - Master Plan**

Project: **City of Prescott Transfer Station**  
 Location: **Sundog Ranch Rd**  
 Revision: Rev 2  
 Client: City of Prescott  
 JRMA #: 6209  
 Date: 2/12/2026

**Site**

Item	Item/Description	Unit	Quantity	Unit Cost	Project Cost
1	Site Grading	LS	1	\$1,000,000	\$ 1,000,000
2	Finished Grade Building Pad	LS	1	\$200,000	\$ 200,000
3	Paving: Concrete	SF	16,000	\$50	\$ 800,000
4	Paving: Asphalt	SY	25,000	\$50	\$ 1,250,000
5	Site Utilities	LS	1	\$800,000	\$ 800,000
6	Site Drainage	LS	1	\$250,000	\$ 250,000
7	Site Signing/Marking	LS	1	\$75,000	\$ 75,000
8	Landscape/Irrigation	LS	1	\$250,000	\$ 250,000
9	Site Boundary Retaining Wall	SF	12,000	\$50	\$ 600,000
10	Retaining Wall	SF	10,000	\$200	\$ 2,000,000
11	Miscellaneous Relocation and/or Demo	LS	1	\$250,000	\$ 250,000
					<b>\$ 7,475,000</b>

**Buildings & Equipment**

Item	Item/Description	Unit	Quantity	Unit Cost	Project Cost
1	TS - PEMB, Primary Building Systems, Push Walls	SF	17,000	\$180	\$ 3,060,000
2	Site Scale and Pit Scale (Loadout Tunnel)	EACH	2	\$150,000	\$ 300,000
3	TS Crane	EACH	1	\$350,000	\$ 350,000
4	Building Renovation (Existing TS into Recycling Center)	SF	3,000	\$100	\$ 300,000
					<b>\$ 4,010,000</b>

**Summary**

<b>Subtotal (Building + Site)</b>				<b>\$ 11,485,000</b>
General Conditions (12 months @ \$100k/mo avg )				\$ 1,200,000
<b>Total Building + Site Construction Costs</b>				<b>\$ 12,685,000</b>

<b>Project Development Costs</b>				
Contractors Fee	5.25%			\$ 665,963
Insurance/Contractors Liability/W.C.	1.00%			\$ 126,850
Engineering fees (Design and Const. Admin)	12.00%			\$ 1,522,200
Escalation (to mid-point of construction)	4.00%			\$ 507,400
Bond (Performance and Payment)	1.25%			\$ 158,563
GC Contingency	5.00%			\$ 634,250
Subtotal 1				\$ 3,615,225
<b>New Subtotal</b>				<b>\$ 16,300,225</b>

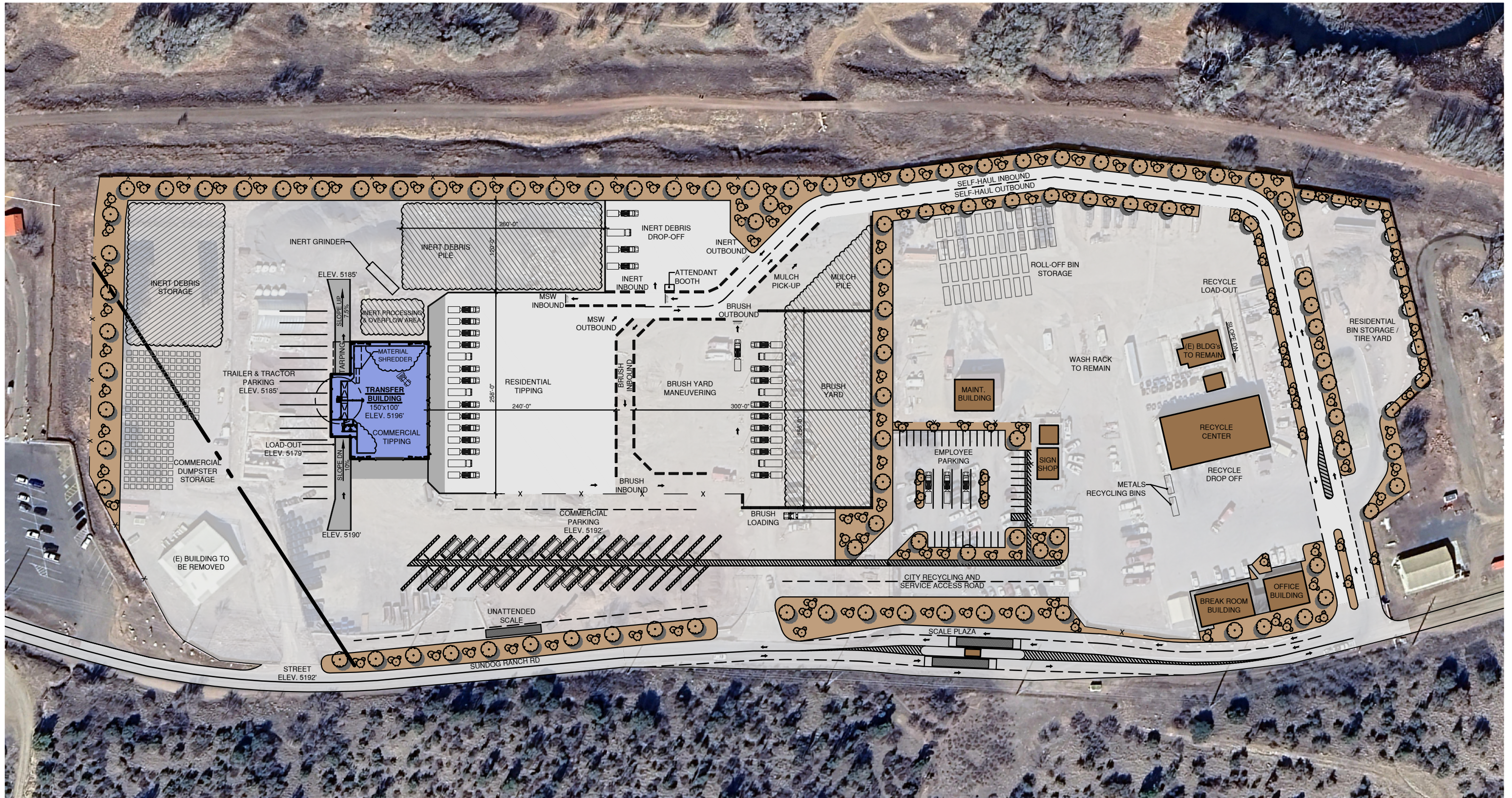
<b>Permits/Fees</b>				
Building Permits and Fees (TBD)	1.00%			\$ 163,002
Utility Connection Fees (TBD)	0.37%			\$ 60,000
Deputy Inspector	0.61%			\$ 100,000
Subtotal 2				\$ 323,002
<b>New Subtotal</b>				<b>\$ 16,623,227</b>

<b>Owner Provided Items</b>				
IT (Data/Security/Cameras/Card Readers/Monitors/AV)	0.60%			\$ 100,000
Signage (TBD)	0.30%			\$ 50,000
FFE (TBD)	0.00%			\$ -
Owner Project Contingency	10.00%			\$ 1,662,323
Subtotal 3				\$ 1,812,323
<b>Total Estimated Project Cost</b>				<b>\$ 18,435,550</b>

**Cost Estimate Notes**

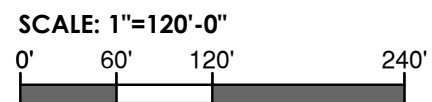
- 1 Rough Order of Magnitude (ROM) is based upon similar projects JRMA has designed
- 2 TS is a standard clear span MBS.
- 3 Does not include remediation for any contaminated soils.
- 4 Escalation added to mid-point of estimated construction duration
- 5 Prevailing Wage is not included in the ROM. Living Wage is included in the ROM.

# Appendix A. Transfer Station Schematic Site Plan Overall Layout (Figure 3-1)



**SITE INFORMATION**

SITE AREA:	977,248 S.F. (APPROX. 22.43 AC.)
BUILDING HEIGHT:	40'-0"
PROPOSED BUILDING FOOTPRINT:	15,000 S.F.
EXISTING BUILDING FOOTPRINT:	17,620 S.F.
PROPOSED LANDSCAPE:	----- S.F.
PROPOSED HARDSCAPE:	271,178 S.F.
UN-DEVELOPED AREA:	255,325 S.F.



SCHEMATIC LAYOUT

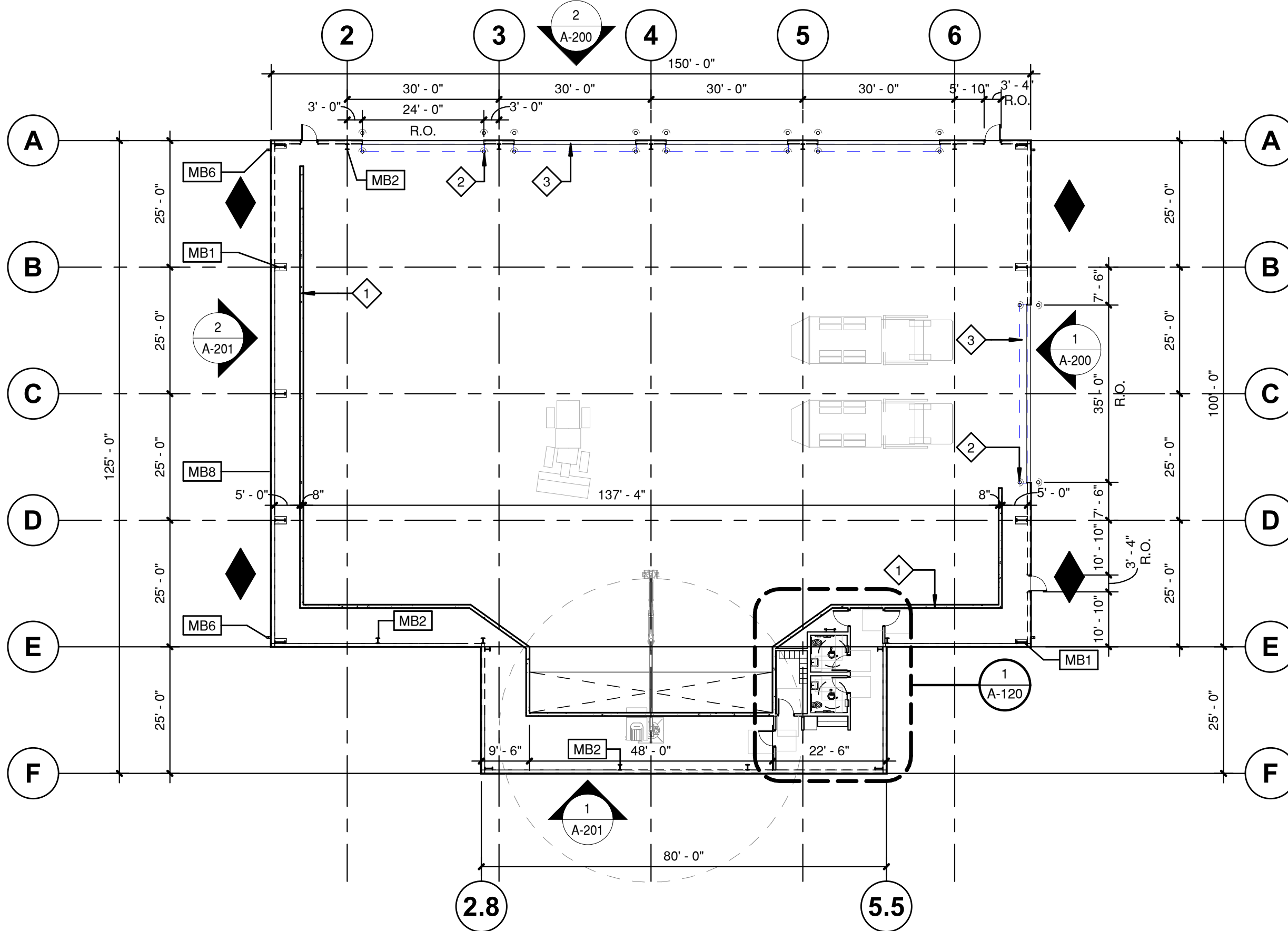
**Prescott Transfer Station**  
 Transfer Station Master Plan  
 2800 Sundog Ranch Road, Prescott, AZ

Job No. 6209  
 02.23.2026

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**A100**

## **Appendix B. New Transfer Station Schematic Floor Plan (Figure 3-2a)**



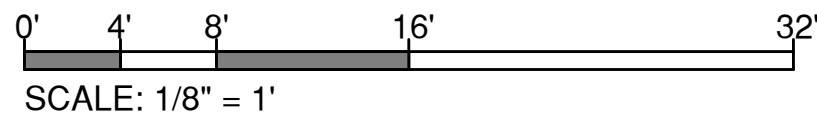
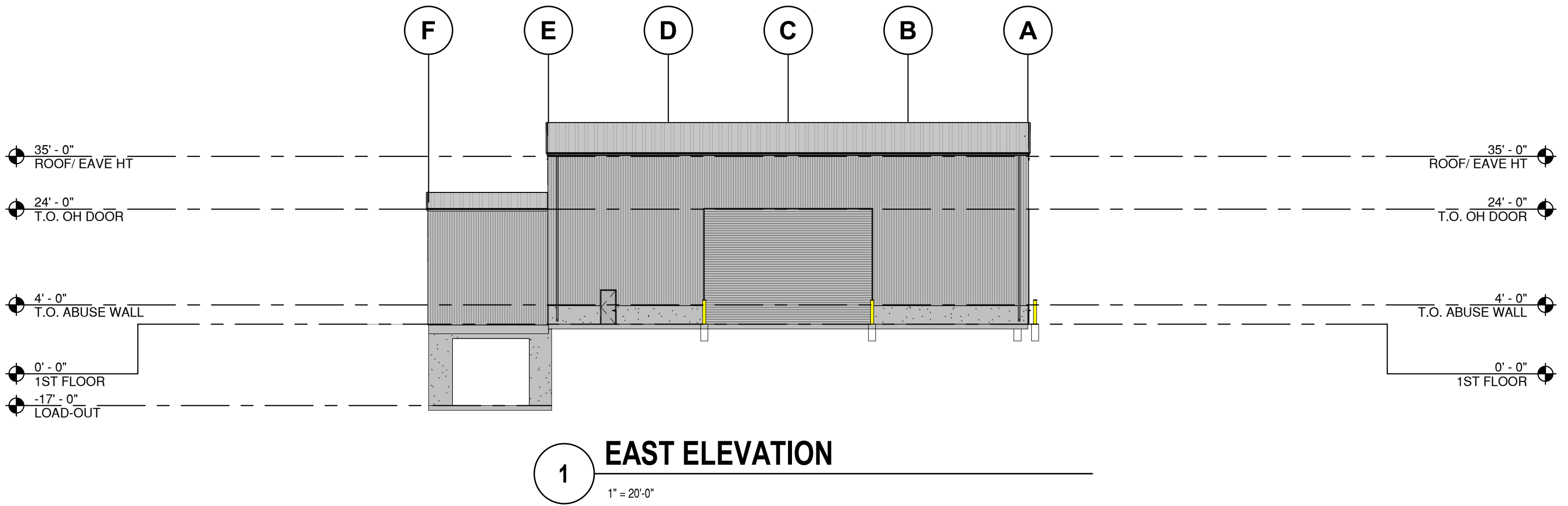
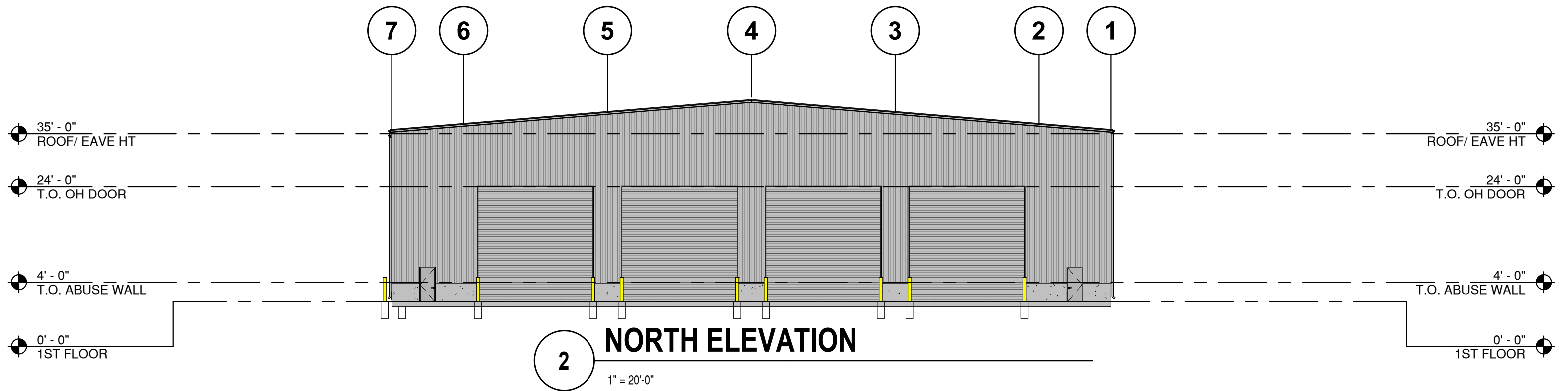
## ◆ KEYNOTES

- 1 CAST IN PLACE CONCRETE WALL
- 2 CONG-FILLED METAL PIPE BOLLARD
- 3 OVERHEAD COILING DOOR

## ▭ MBS KEYNOTES

- MB1 RIGID FRAME COLUMN (MBS)
- MB2 END WALL COLUMN (MBS)
- MB4 GUTTER (MBS)
- MB5 RAKE TRIM (MBS)
- MB6 DOWNSPOUT (MBS)
- MB7 METAL ROOF (MBS)
- MB8 METAL WALL PANEL

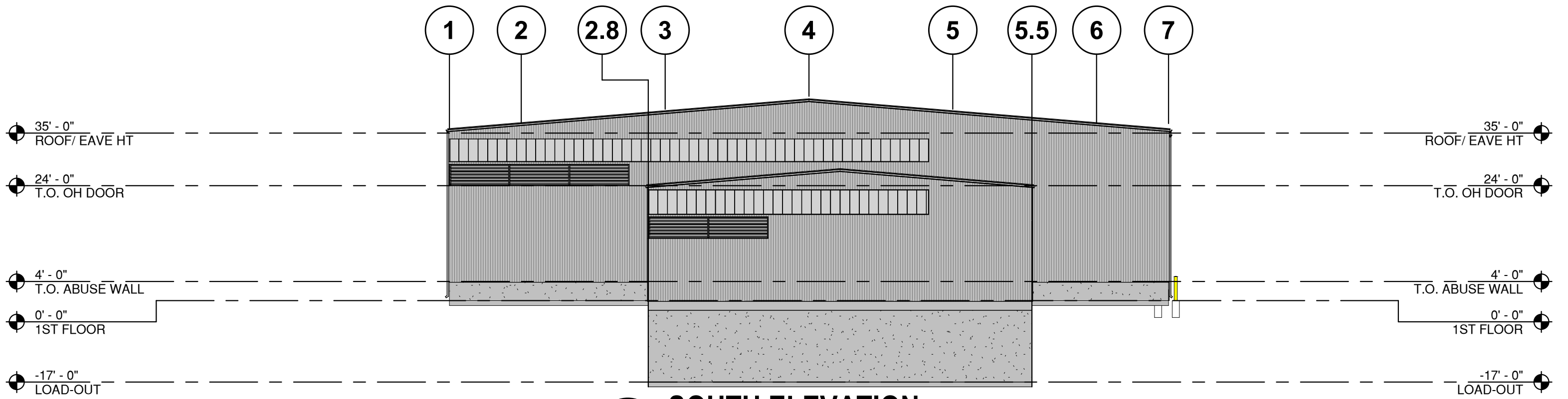
# Appendix C. Transfer Station North and East Elevations (Figure 3-2b)



EXTERIOR ELEVATIONS  
**Prescott Transfer Station**  
 Transfer Station Master Plan  
 2800 Sundog Ranch Road, Prescott, AZ

JRMA Job No. 6209  
 01.30.2026  
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**A-200**  
 Page 47 of 90

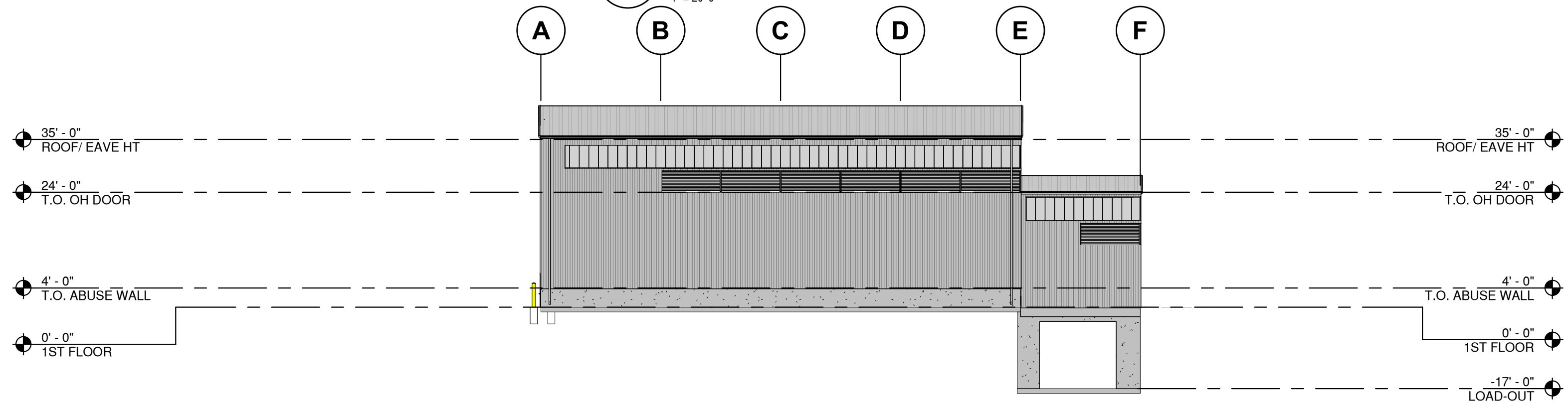
**Appendix D. Transfer Station South and West Elevations  
(Figure 3-2c)**



**SOUTH ELEVATION**

1

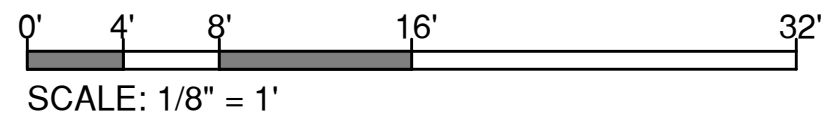
1" = 20'-0"



**WEST ELEVATION**

2

1" = 20'-0"



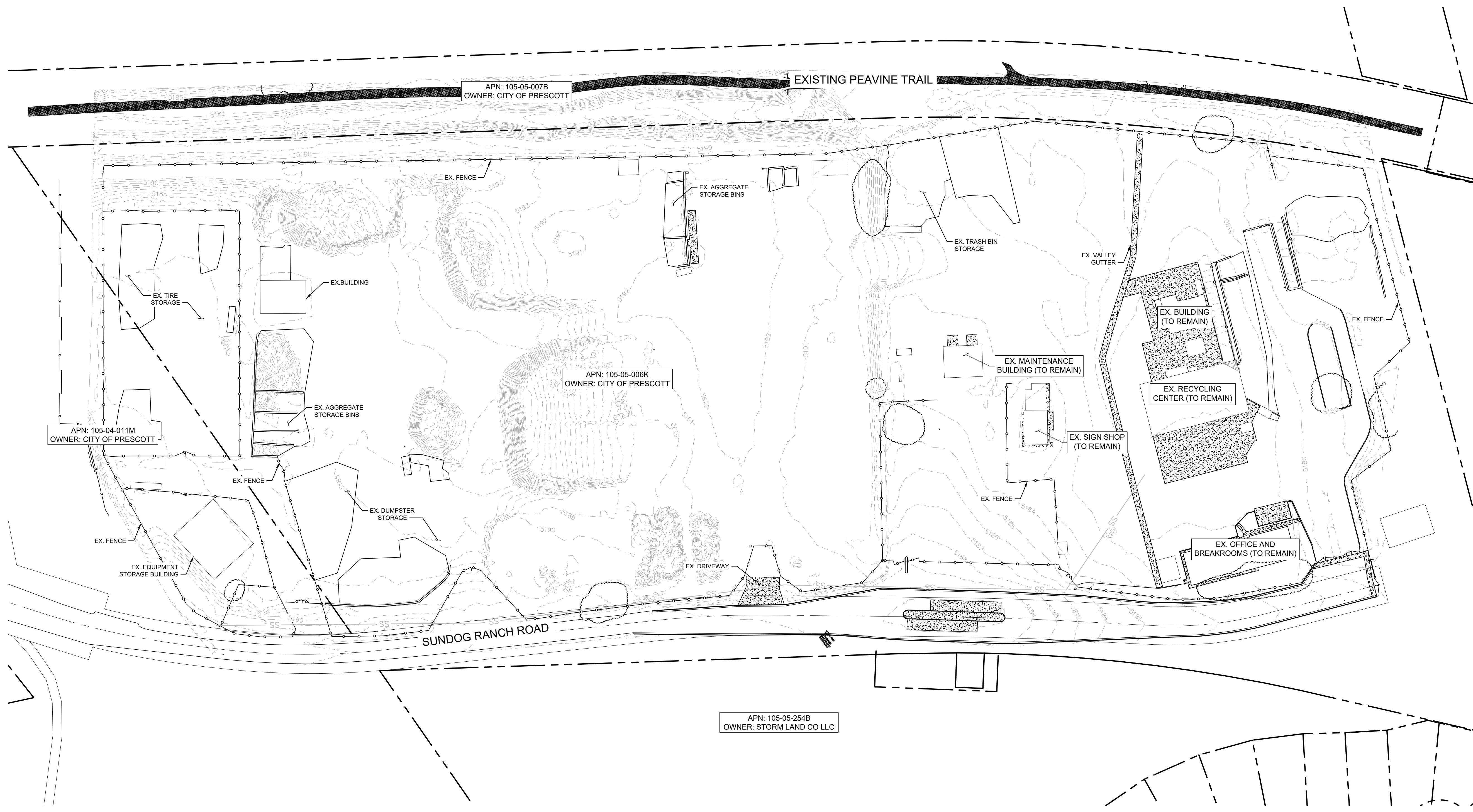
EXTERIOR ELEVATIONS

**Prescott Transfer Station**  
 Transfer Station Master Plan  
 2800 Sundog Ranch Road, Prescott, AZ

JRMA Job No. 6209  
 01.30.2026  
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**A-201**

# Appendix E. Site Topographic Survey (Figure 3-3a)



Call at least two full working days before you begin excavation.

**ARIZONA 811**  
Arizona Blue Stakes, Inc.

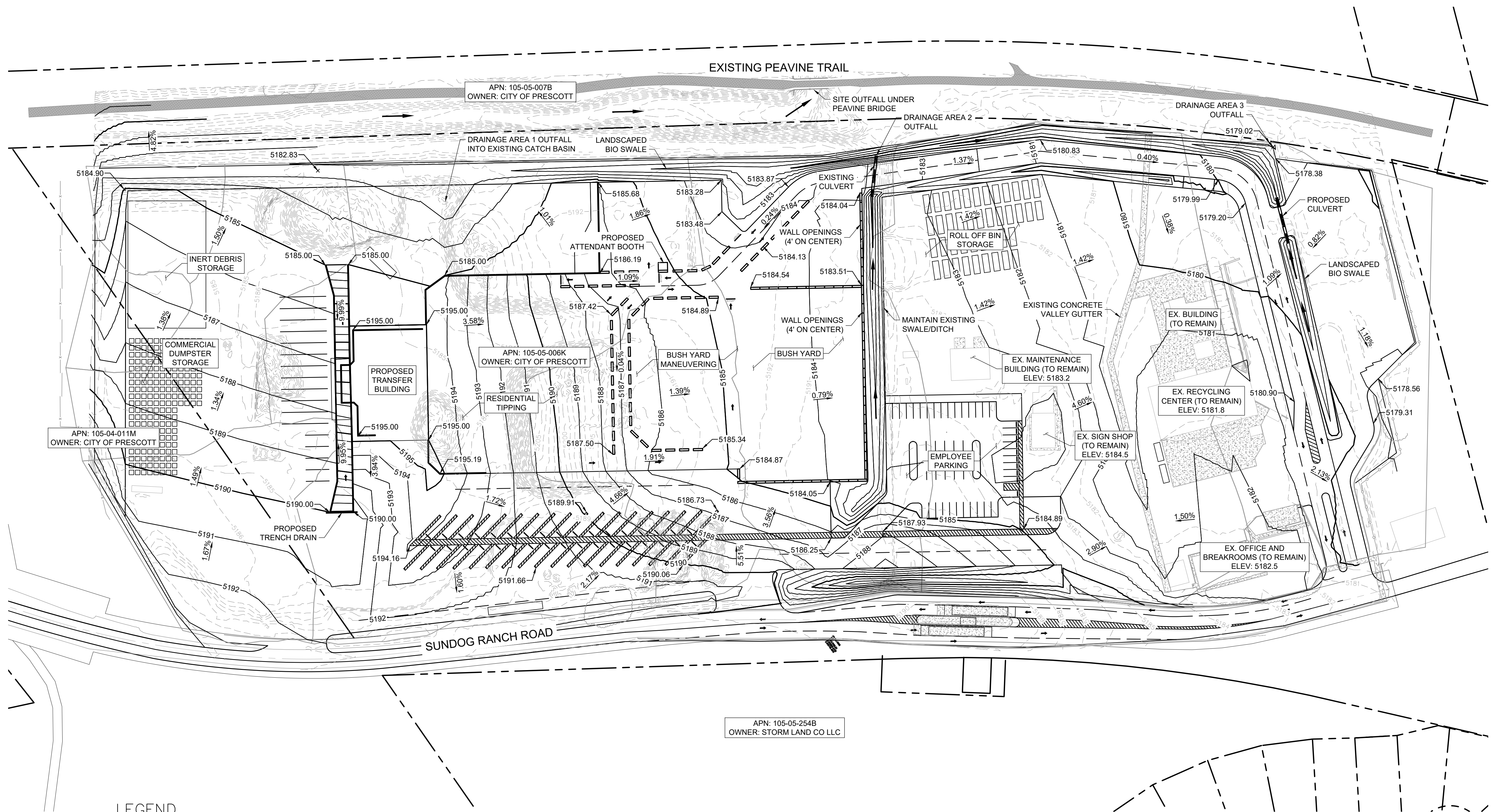
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)  
In Maricopa County: (602) 263-1100

GRAPHIC SCALE IN FEET  
0 35 70 140

NORTH

CITY OF PRESCOTT PUBLIC WORKS		DWG. NO.	01	OF	01
PRESCOTT TRANSFER STATION MASTER PLAN		DATE	1/26/2026		
EXISTING CONDITIONS		SHEET NO.	01		
CITY OF PRESCOTT		PREPARED BY	ALH	CHECKED BY	ACB
		FOR:	COP		
Kimley»Horn		© 2025 KIMLEY-HORN & ASSOCIATES, INC. SUITE 303 101 W. GOODWIN ST. SUITE 303 PHOENIX, AZ 85003 PHONE: 928-448-9700 WWW.KIMLEY-HORN.COM			
NO.	BY	DATE	REVISIONS	APPRV/ DATE	CITY
	ENGINEER				
REVIEW:					

## Appendix F. Conceptual Site Grading Plan (Figure 3-3b)

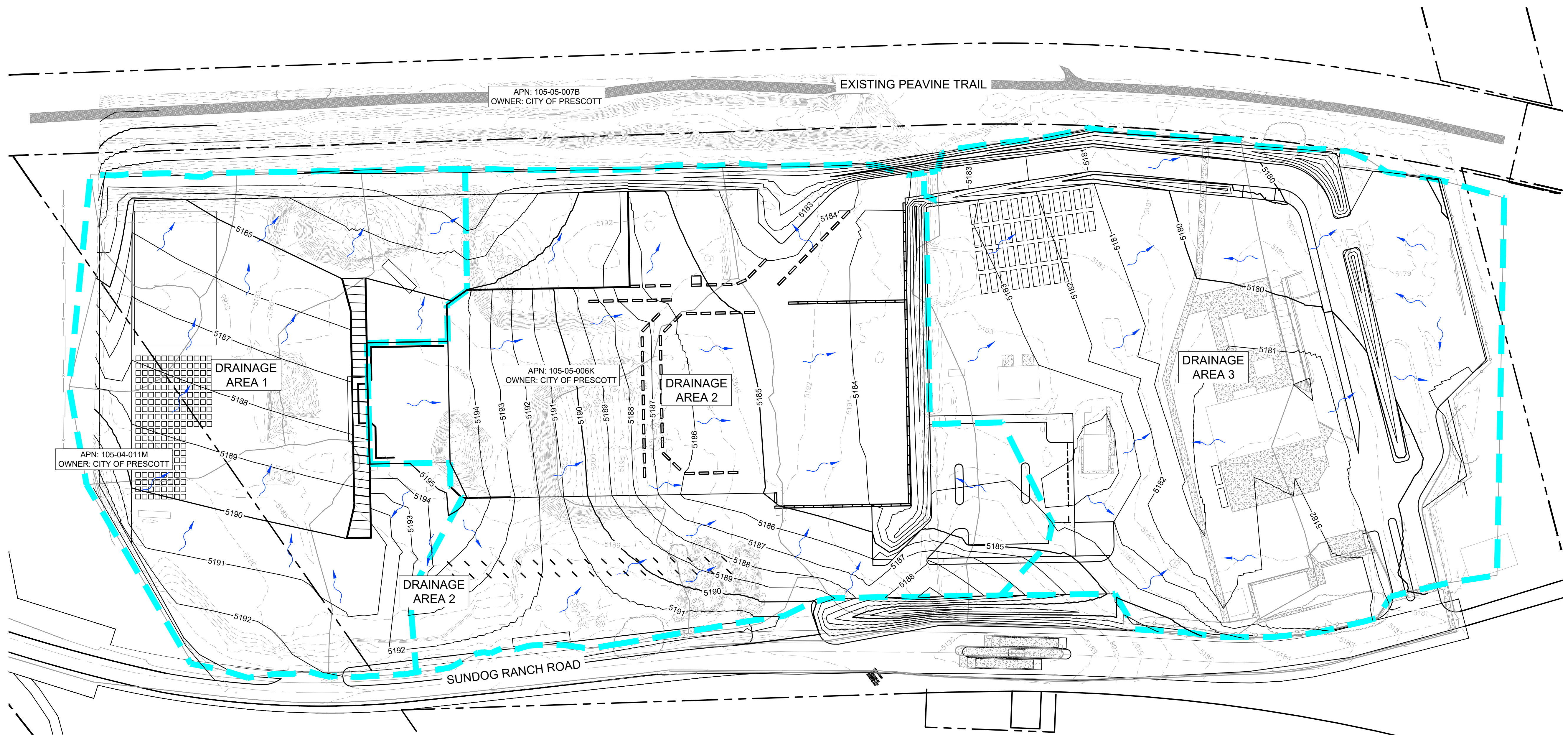


**LEGEND**  
XXXX.XX FINISHED GRADE SURFACE ELEVATION

**Call at least two full working days before you begin excavation.**  
**ARIZONA 811**  
Arizona Missions, Inc.  
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)  
In Maricopa County: (602) 263-1100

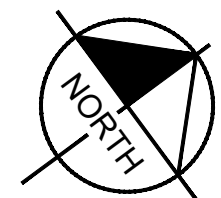
CITY OF PRESCOTT PUBLIC WORKS		DWG. NO.	01	OF	01
<b>PRESCOTT TRANSFER STATION MASTER PLAN GRADING EXHIBIT</b>		DATE	2/12/2026		
CITY OF PRESCOTT		SHEET NO.	01		
PREPARED BY:	ALH	CHECKED BY:	ACB	FOR:	COP
Kimley-Horn & Associates, Inc. 101 W. GOVERN ST., SUITE 303 PHOENIX, AZ 85004 PHONE: 602-498-9700 WWW.KIMLEY-HORN.COM		APPROV. DATE		REVISIONS	
NO.	BY	DATE		ENGINEER	
REVIEW:					

# Appendix G. Conceptual Site Drainage Plan (Figure 3-3c)



**LEGEND**

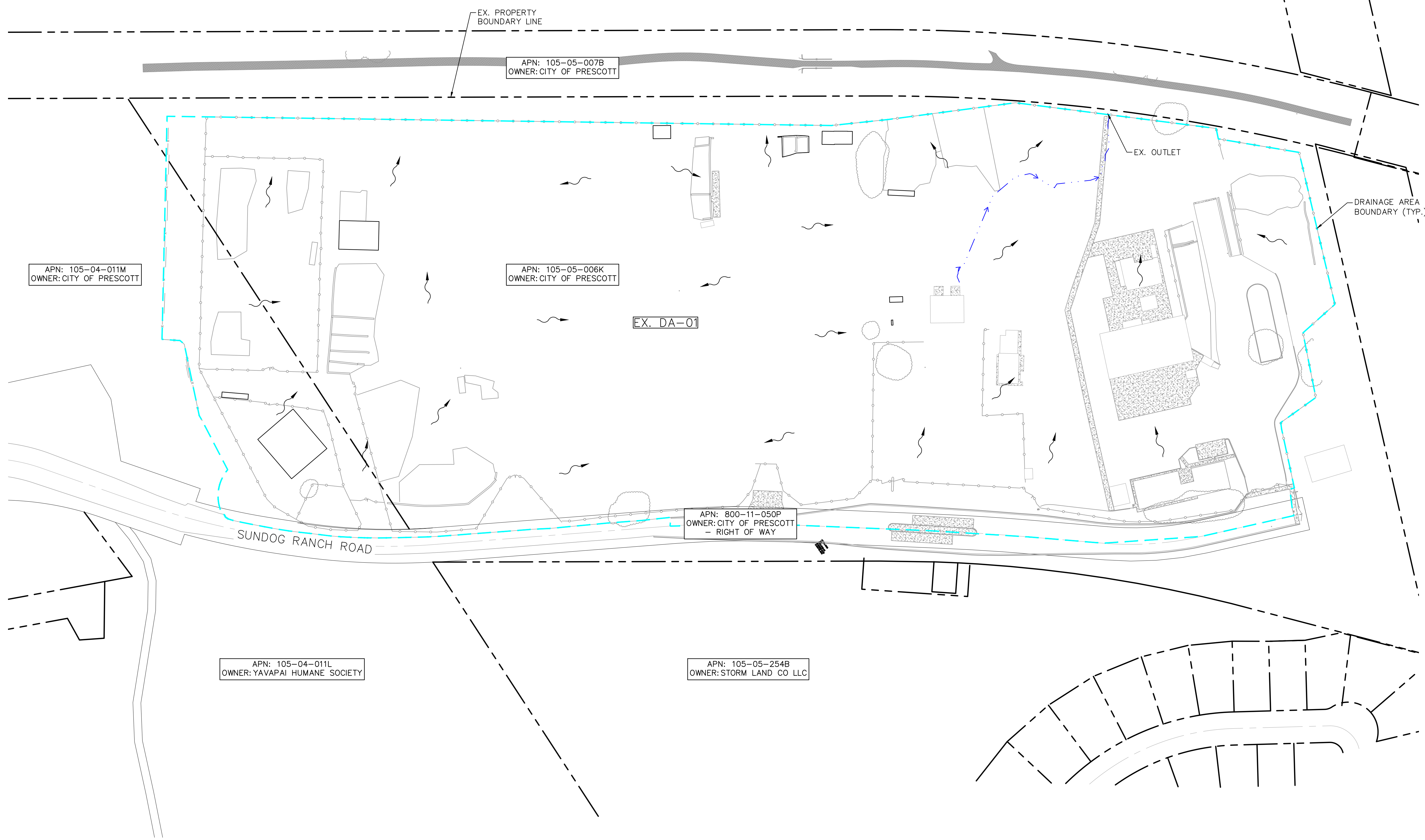
- - - - - DRAINAGE BASIN AREA
- EXISTING AND PROPOSED CONTOURS SHOWN AT 1-FT INTERVALS



<b>Kimley»Horn</b> <small>© 2025 KIMLEY-HORN &amp; ASSOCIATES, INC. 101 W. GOODWIN ST., SUITE 303 PHOENIX, AZ 85003 PHONE: 602-448-9700 WWW.KIMLEY-HORN.COM</small>		<b>CITY OF PRESCOTT</b>	CITY OF PRESCOTT PUBLIC WORKS	DWG. NO.
APPROV. DATE CITY		PREPARED BY: ALH		DATE: 1/26/2026
REVISIONS		CHECKED BY: ACB		SHEET NO. 01
NO. BY DATE ENGINEER		FOR: COP		OF 01
REVIEW:		PROJECT NAME		
		<b>PRESCOTT TRANSFER STATION MASTER PLAN DRAINAGE EXHIBIT</b>		

# Appendix H. Preliminary Drainage Rational Method

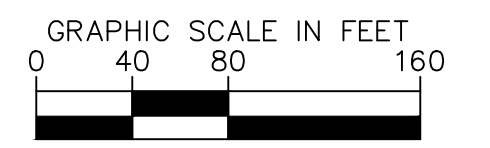
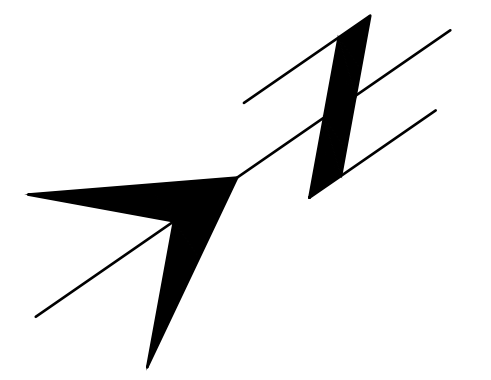
K:\P\RS\_Roadway\232134000 - Transfer Station MPE\Engineering\Drawings\Calculations\Drainage\Exhibit.dwg - Existing DA - Jan 28, 2026 10:47am by: Ethan Hopp



**LEGEND**

- DA-#N DRAINAGE BASIN ID
- - - - DRAINAGE BASIN AREA
- - - - FLOW PATH

EXISTING CONTOURS SHOWN AT 2-FT INTERVALS

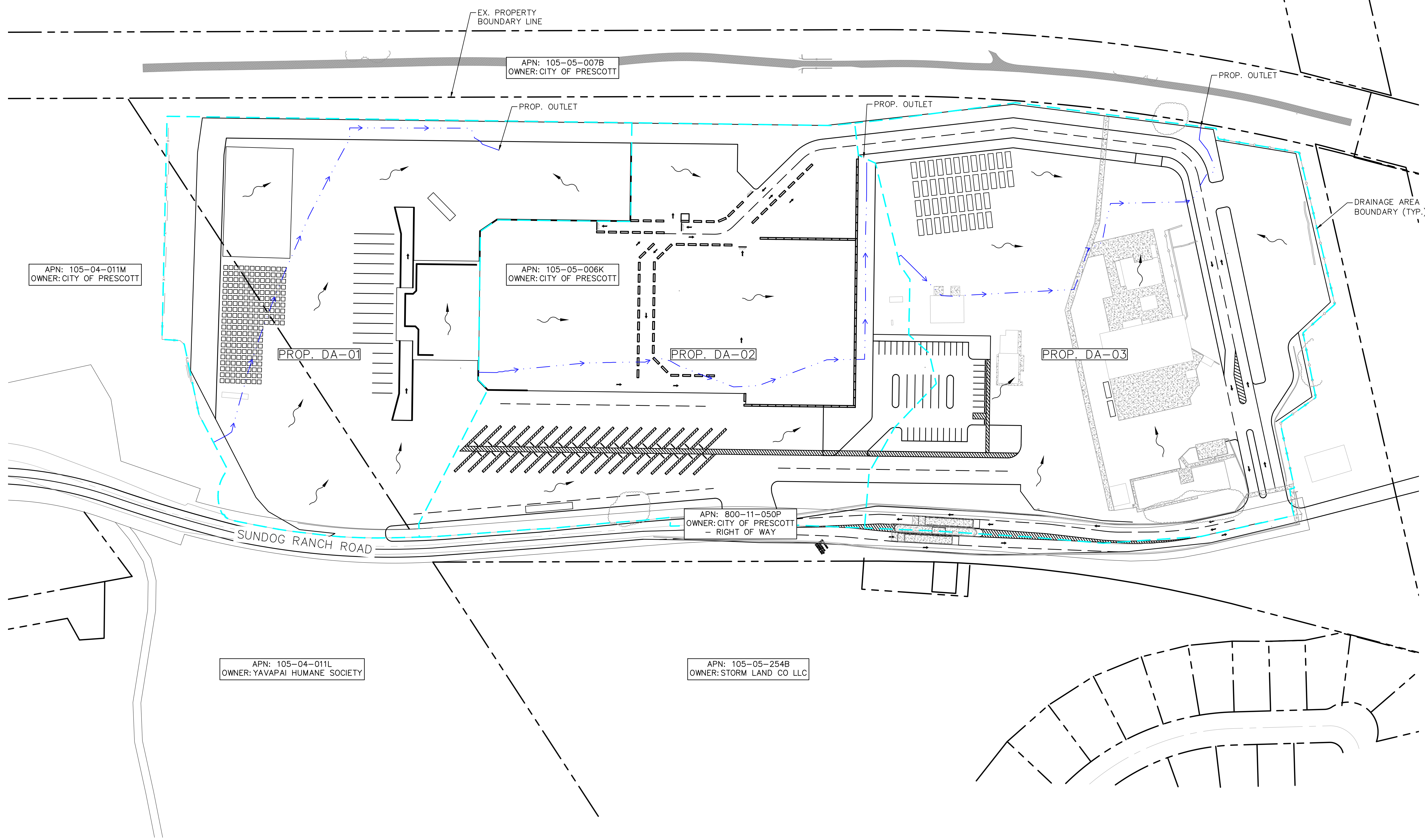


01/27/2026



# TRANSFER STATION MASTER PLAN - EXISTING DRAINAGE AREA MAP

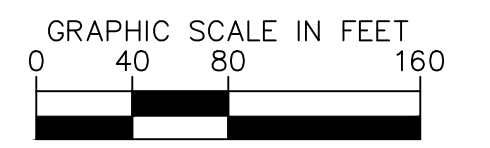
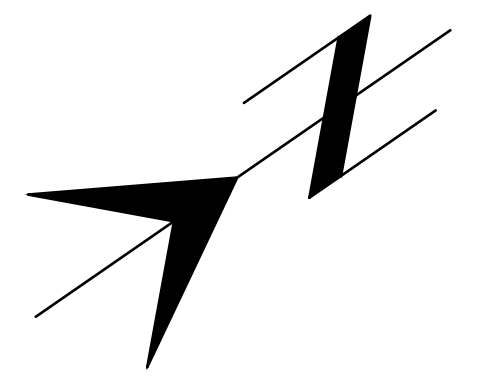
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**LEGEND**

- DA-#N DRAINAGE BASIN ID
- DRAINAGE BASIN AREA
- FLOW PATH

EXISTING AND PROPOSED CONTOURS SHOWN AT 1-FT INTERVALS



01/27/2026



# TRANSFER STATION MASTER PLAN - PROPOSED DRAINAGE AREA MAP

General Project Information			
Project	Transfer Station MP		
Project #	-		
Designed by	EKH	Date	1/27/2026

NOAA 14 Rainfall Depth Data [in]										
	Storm Event [yr]									
Duration	1	2	5	10	25	50	100	200	500	1000
5-min:	0.246	0.319	0.432	0.523	0.653	0.759	0.873	0.994	1.17	1.32
10-min:	0.374	0.485	0.657	0.796	0.995	1.16	1.33	1.51	1.78	2
15-min:	0.464	0.602	0.815	0.987	1.23	1.43	1.65	1.88	2.2	2.48
30-min:	0.625	0.81	1.1	1.33	1.66	1.93	2.22	2.52	2.97	3.34
60-min:	0.773	1	1.36	1.64	2.06	2.39	2.74	3.12	3.67	4.13
2-hr:	0.899	1.14	1.51	1.82	2.26	2.63	3.02	3.44	4.06	4.57
3-hr:	0.966	1.22	1.58	1.88	2.31	2.67	3.06	3.49	4.12	4.63
6-hr:	1.15	1.43	1.8	2.1	2.55	2.91	3.3	3.71	4.3	4.79
12-hr:	1.43	1.78	2.19	2.52	2.98	3.34	3.7	4.08	4.6	5.04
24-hr:	1.74	2.19	2.75	3.2	3.8	4.28	4.77	5.26	5.93	6.46
2-day:	2.01	2.52	3.18	3.71	4.43	5	5.58	6.19	7.01	7.66
3-day:	2.15	2.7	3.41	3.97	4.73	5.33	5.95	6.58	7.44	8.11
4-day:	2.29	2.88	3.64	4.23	5.04	5.67	6.32	6.98	7.88	8.56
7-day:	2.77	3.48	4.38	5.09	6.04	6.78	7.53	8.29	9.3	10.1
10-day:	3.08	3.87	4.88	5.66	6.69	7.48	8.28	9.07	10.1	10.9
20-day:	4.14	5.22	6.49	7.42	8.57	9.41	10.2	11	11.9	12.6
30-day:	4.94	6.23	7.8	8.97	10.4	11.5	12.6	13.6	14.9	15.9
45-day:	5.95	7.52	9.48	10.9	12.8	14.2	15.6	17	18.7	20
60-day:	6.75	8.55	10.8	12.4	14.4	16	17.4	18.8	20.7	22

NOAA 14 Rainfall Intensity [in/hr]										
	Storm Event									
Duration	1	2	5	10	25	50	100	200	500	1000
5-min:	2.95	3.83	5.18	6.28	7.84	9.11	10.48	11.93	14.04	15.84
10-min:	2.24	2.91	3.94	4.78	5.97	6.96	7.98	9.06	10.68	12.00
15-min:	1.86	2.41	3.26	3.95	4.92	5.72	6.60	7.52	8.80	9.92
30-min:	1.25	1.62	2.20	2.66	3.32	3.86	4.44	5.04	5.94	6.68
60-min:	0.77	1.00	1.36	1.64	2.06	2.39	2.74	3.12	3.67	4.13
2-hr:	0.45	0.57	0.76	0.91	1.13	1.32	1.51	1.72	2.03	2.29
3-hr:	0.32	0.41	0.53	0.63	0.77	0.89	1.02	1.16	1.37	1.54
6-hr:	0.19	0.24	0.30	0.35	0.43	0.49	0.55	0.62	0.72	0.80
12-hr:	0.119	0.148	0.183	0.210	0.248	0.278	0.308	0.340	0.383	0.420
24-hr:	0.073	0.091	0.115	0.133	0.158	0.178	0.199	0.219	0.247	0.269
2-day:	0.042	0.053	0.066	0.077	0.092	0.104	0.116	0.129	0.146	0.160
3-day:	0.030	0.038	0.047	0.055	0.066	0.074	0.083	0.091	0.103	0.113
4-day:	0.024	0.030	0.038	0.045	0.053	0.060	0.067	0.073	0.083	0.090
7-day:	0.016	0.021	0.026	0.030	0.036	0.040	0.045	0.049	0.055	0.060
10-day:	0.013	0.016	0.020	0.024	0.028	0.031	0.035	0.038	0.042	0.045
20-day:	0.009	0.011	0.014	0.015	0.018	0.020	0.021	0.023	0.025	0.026
30-day:	0.007	0.009	0.011	0.012	0.014	0.016	0.018	0.019	0.021	0.022
45-day:	0.006	0.007	0.009	0.010	0.012	0.013	0.014	0.016	0.017	0.019
60-day:	0.005	0.006	0.008	0.009	0.010	0.011	0.012	0.013	0.014	0.015

General Project Information			
Project #	Transfer Station MP		
Designed by	EKH	Date	1/27/26
Design Storm Event [yr]	100		
Duration [hr]	2		

Drainage Area	Land Use	Area [sf]	Area [ac]	Rational Coefficient	Precipitation Depth (in)	Volume Required [cf]	Volume Required [ac-ft]
DA-01	Impervious	235959	5.42	0.95	3.02	56414	1.30
DA-01	Pervious	810461	18.61	0.61	3.02	124419	2.86
DA-01 Total						180833	4.15

General Project Information			
Project #	Transfer Station MP		
Designed by	EKH	Date	1/27/26
Design Storm Event [yr]	100		
Duration [hr]	2		

Drainage Area	Land Use	Area [sf]	Area [ac]	Rational Coefficient	Precipitation Depth (in)	Volume Required [cf]	Volume Required [ac-ft]
DA-01	Impervious	242724	5.57	0.95	3.02	58031	1.33
DA-01	Pervious	60402	1.39	0.61	3.02	9273	0.21
DA-02	Impervious	307821	7.07	0.95	3.02	73595	1.69
DA-02	Pervious	45931	1.05	0.61	3.02	7051	0.16
DA-03	Impervious	331141	7.60	0.95	3.02	79170	1.82
DA-03	Pervious	58401	1.34	0.61	3.02	8966	0.21
DA-01 Total						67304	1.55
DA-02 Total						80646	1.85
DA-03 Total						86221	1.98
Total						234171	5.38

## First Flush Volume

General Project Information			
Project #	Transfer Station MP		
Designed by	EKH	Date	1/27/26
Design Storm Event [yr]	100		
Duration [hr]	2		

Drainage Area	Land Use	Area [sf]	Area [ac]	Rational Coefficient	Precipitation Depth (in)	Volume Required [cf]	Volume Required [ac-ft]
DA-01	Impervious	242724	5.57	0.95	0.50	9608	0.22
DA-01	Pervious	60402	1.39	0.61	0.50	1535	0.04
DA-02	Impervious	307821	7.07	0.95	0.50	12185	0.28
DA-02	Pervious	45931	1.05	0.61	0.50	1167	0.03
DA-03	Impervious	331141	7.60	0.95	0.50	13108	0.30
DA-03	Pervious	58401	1.34	0.61	0.50	1484	0.03
First Flush DA-01 Total						11143	0.26
First Flush DA-02 Total						13352	0.31
First Flush DA-03 Total						14275	0.33
First Flush Total						38770	0.89

General Project Information			
Project #	Transfer Station MP		
Designed by	EKH	Date	1/27/2026
Design Storm Event		2	
Minimum $T_c$ [min]		5	

Drainage Area Information					Hydrology				
Drainage Area	Longitudinal Slope, $S_l$ [ft/ft]	Rational Coefficient	Flowpath Length [ft]	Area [ac]	FCDMC Resistance Coefficient Type	$K_b$	$I$ [in/hr]	$T_c$ [min]	$Q$ [cfs]
EX. DA-01	0.010	0.69	430	24.02	C	0.115	2.61	12.9	43.03
DA-01	0.015	0.88	770	6.96	C	0.129	2.30	16.9	14.14
DA-02	0.015	0.91	900	8.12	C	0.127	2.25	18.3	16.55
DA-03	0.010	0.90	700	8.94	C	0.126	2.25	18.2	18.09

General Project Information			
Project #	Transfer Station MP		
Designed by	EKH	Date	1/27/2026
Design Storm Event		10	
Minimum $T_c$ [min]		5	

Drainage Area Information					Hydrology				
Drainage Area	Longitudinal Slope, $S_l$ [ft/ft]	Rational Coefficient	Flowpath Length [ft]	Area [ac]	FCDMC Resistance Coefficient Type	$K_b$	$I$ [in/hr]	$T_c$ [min]	$Q$ [cfs]
EX. DA-01	0.010	0.69	430	24.02	C	0.115	4.78	10.3	78.78
DA-01	0.015	0.88	770	6.96	C	0.129	4.11	13.6	25.26
DA-02	0.015	0.91	900	8.12	C	0.127	3.95	14.8	29.04
DA-03	0.010	0.90	700	8.94	C	0.126	3.95	14.7	31.74

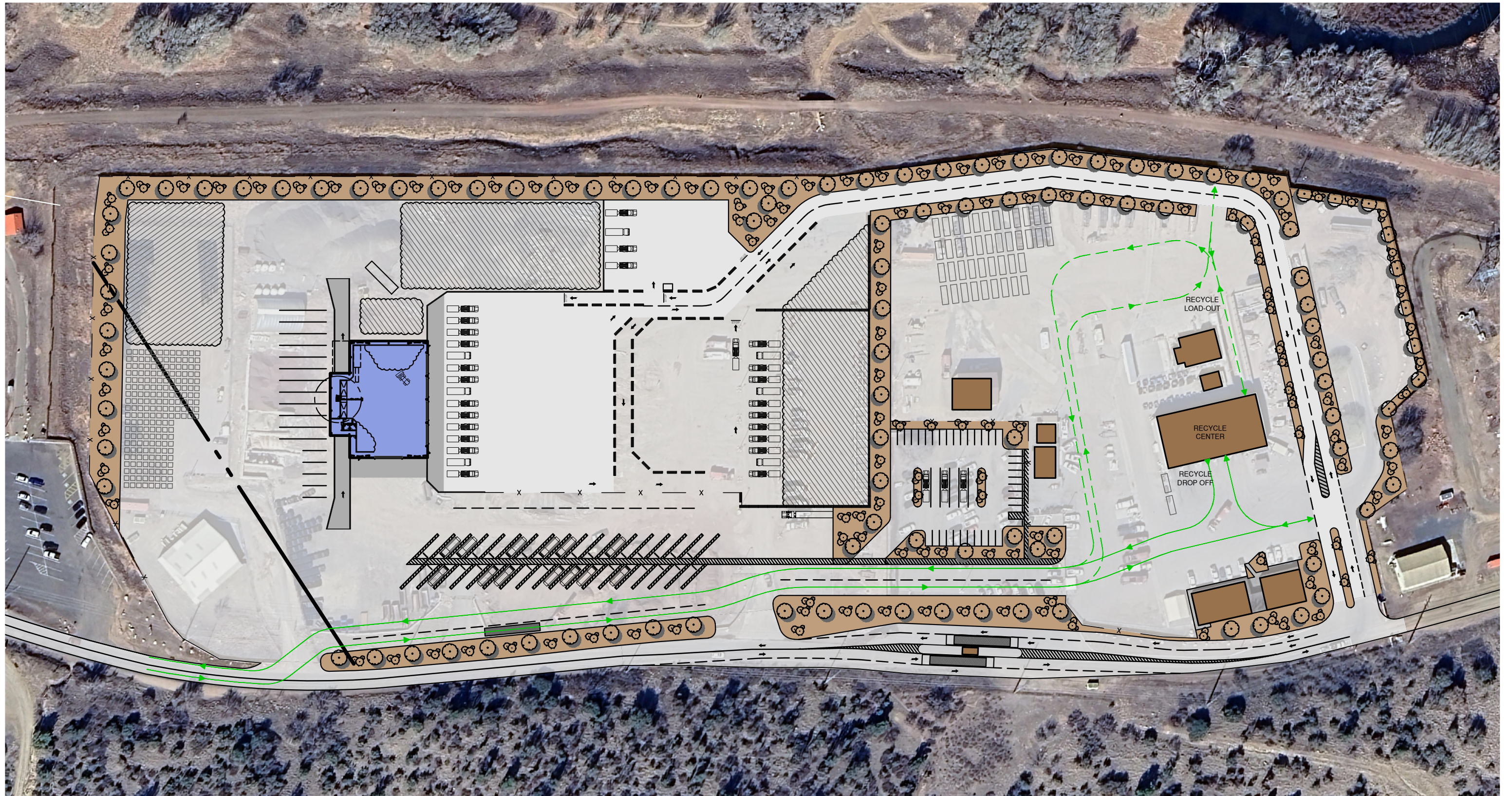
General Project Information			
Project #	Transfer Station MP		
Designed by	EKH	Date	1/27/2026
Design Storm Event		25	
Minimum $T_c$ [min]		5	

Drainage Area Information					Hydrology				
Drainage Area	Longitudinal Slope, $S_l$ [ft/ft]	Rational Coefficient	Flowpath Length [ft]	Area [ac]	FCDMC Resistance Coefficient Type	$K_b$	$I$ [in/hr]	$T_c$ [min]	$Q$ [cfs]
EX. DA-01	0.010	0.69	430	24.02	C	0.115	6.34	9.2	104.63
DA-01	0.015	0.88	770	6.96	C	0.129	5.55	12.1	34.07
DA-02	0.015	0.91	900	8.12	C	0.127	5.34	13.2	39.28
DA-03	0.010	0.90	700	8.94	C	0.126	5.34	13.1	42.93

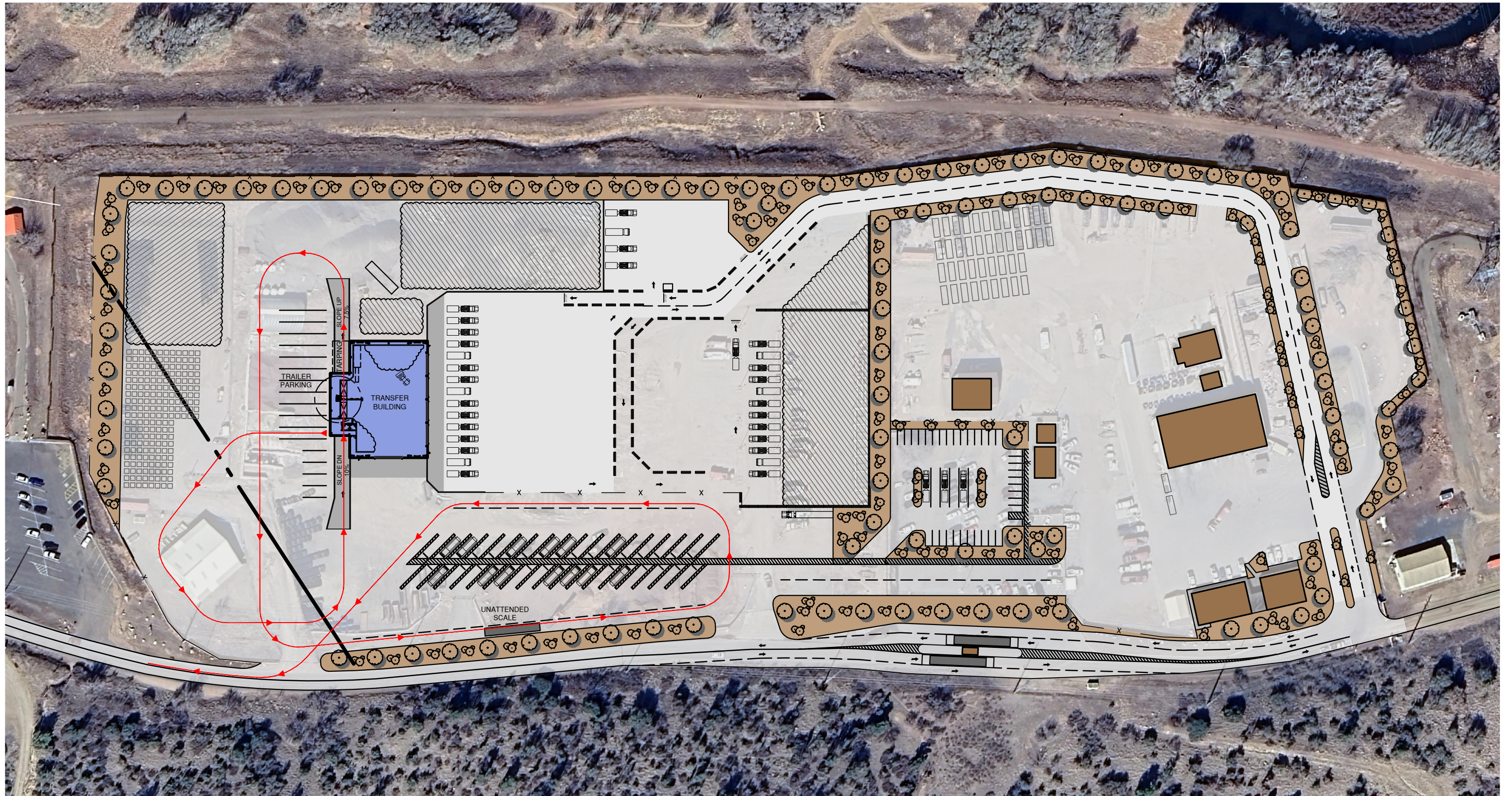
General Project Information			
Project #	Transfer Station MP		
Designed by	EKH	Date	1/27/2026
Design Storm Event		100	
Minimum $T_c$ [min]		5	

Drainage Area Information					Hydrology				
Drainage Area	Longitudinal Slope, $S_l$ [ft/ft]	Rational Coefficient	Flowpath Length [ft]	Area [ac]	FCDMC Resistance Coefficient Type	$K_b$	$I$ [in/hr]	$T_c$ [min]	$Q$ [cfs]
EX. DA-01	0.010	0.69	430	24.02	C	0.115	8.98	8.1	148.10
DA-01	0.015	0.88	770	6.96	C	0.129	7.70	10.7	47.30
DA-02	0.015	0.91	900	8.12	C	0.127	7.43	11.6	54.64
DA-03	0.010	0.90	700	8.94	C	0.126	7.43	11.6	59.72

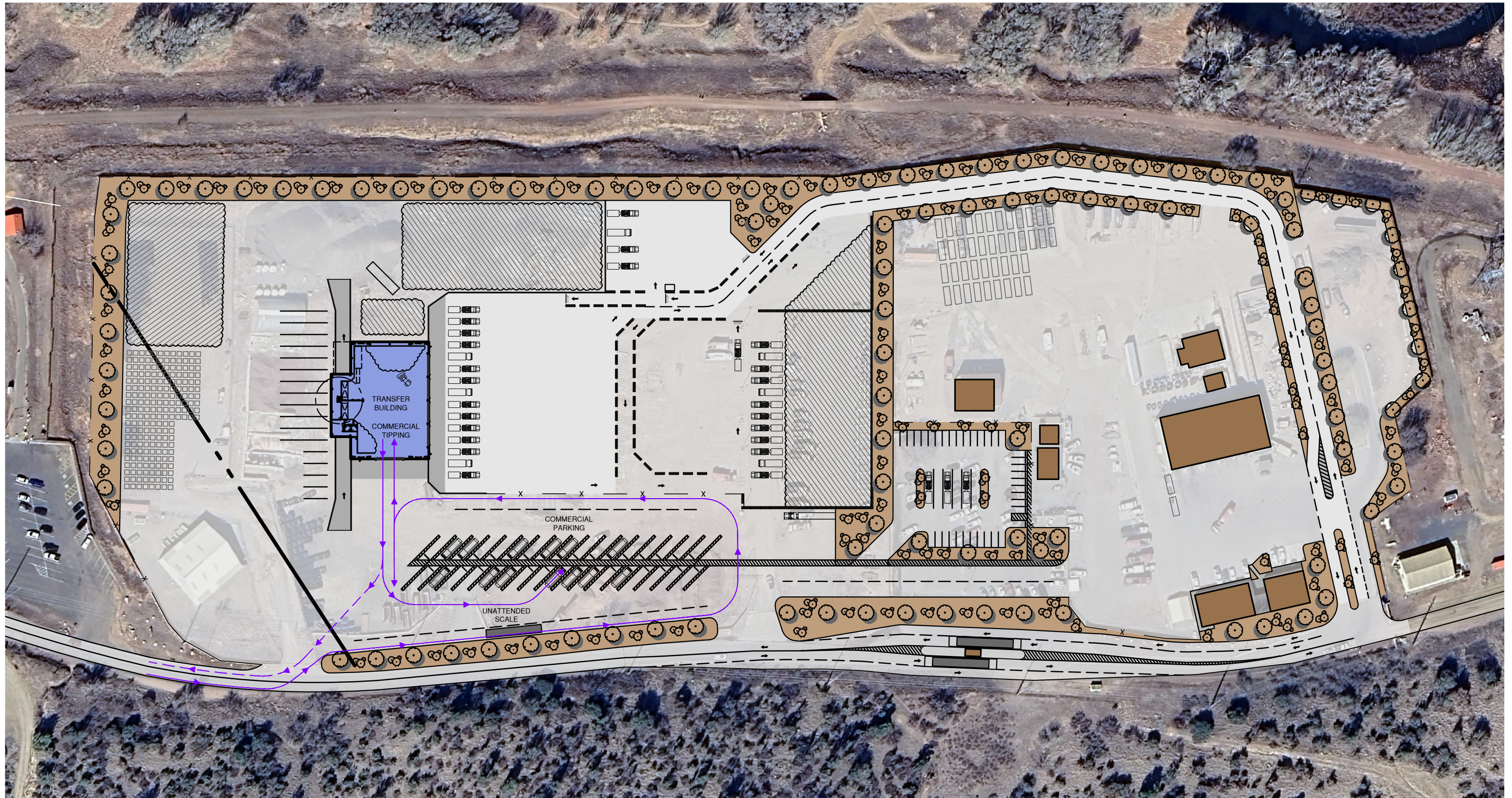
**Appendix I. Schematic Site Plan - Recycling Route  
(Figure 3-4a)**



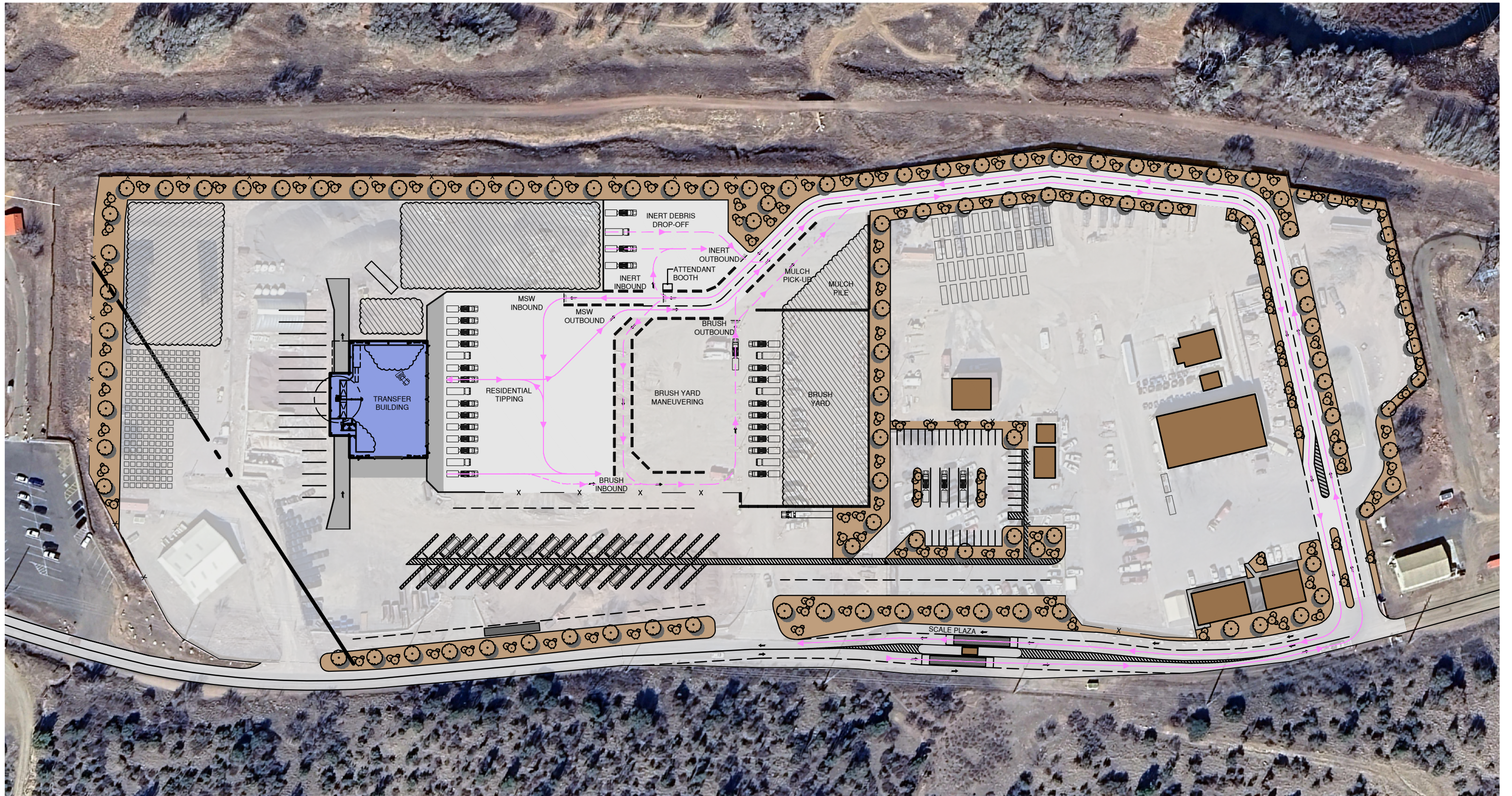
## Appendix J. Schematic Site Plan – Transfer Truck Route (Figure 3-4b)



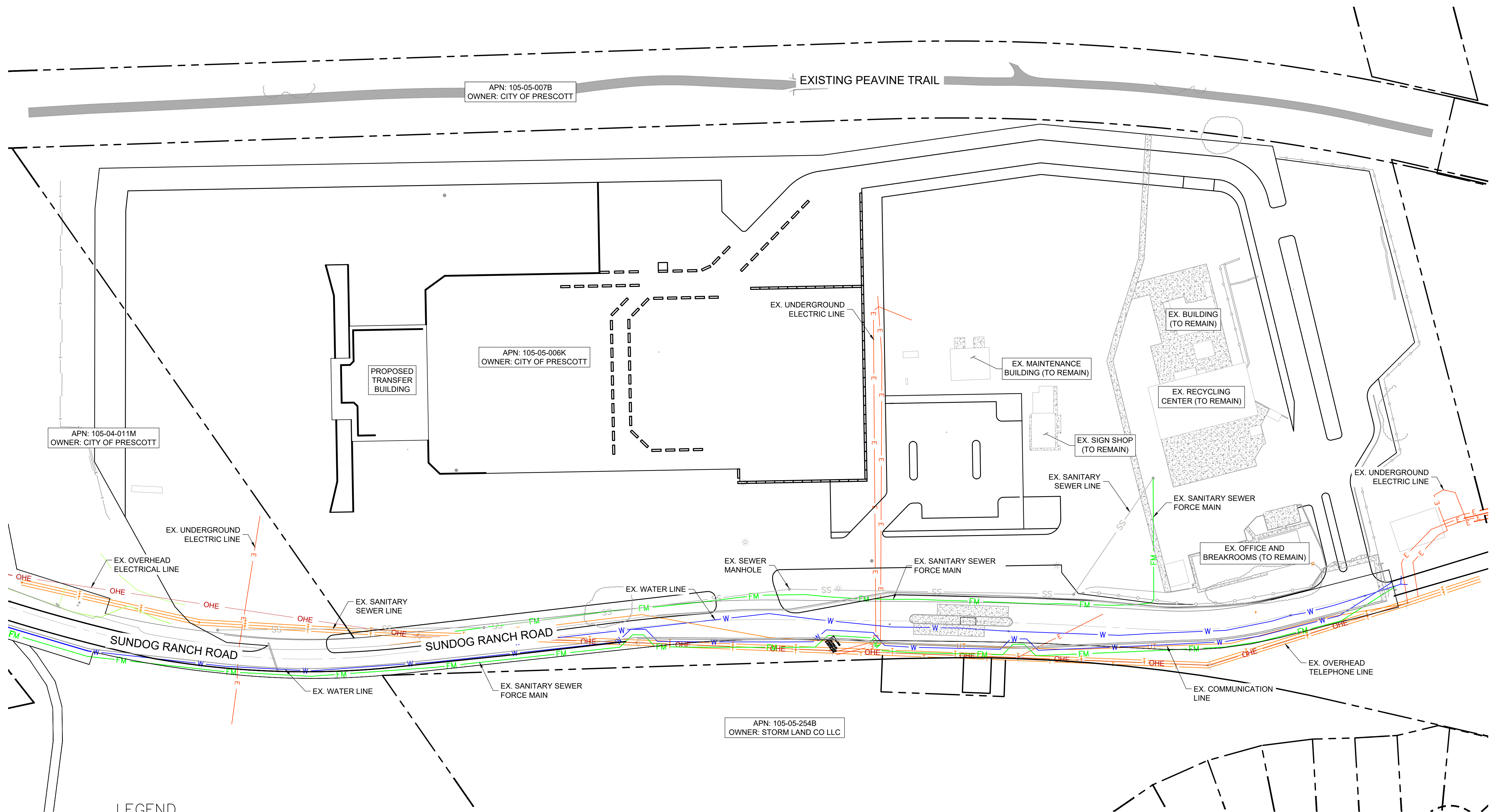
**Appendix K. Schematic Site Plan – Commercial MSW  
Route (Figure 3-4c)**



# Appendix L. Schematic Site Plan – Self Haul Route (Figure 3-4d)

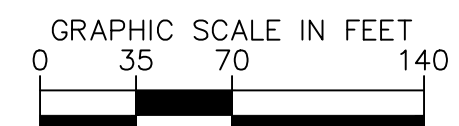
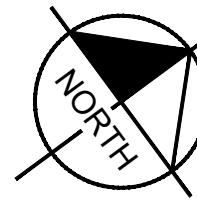


# Appendix M. Site Utility Plan (Figure 4-1-4)



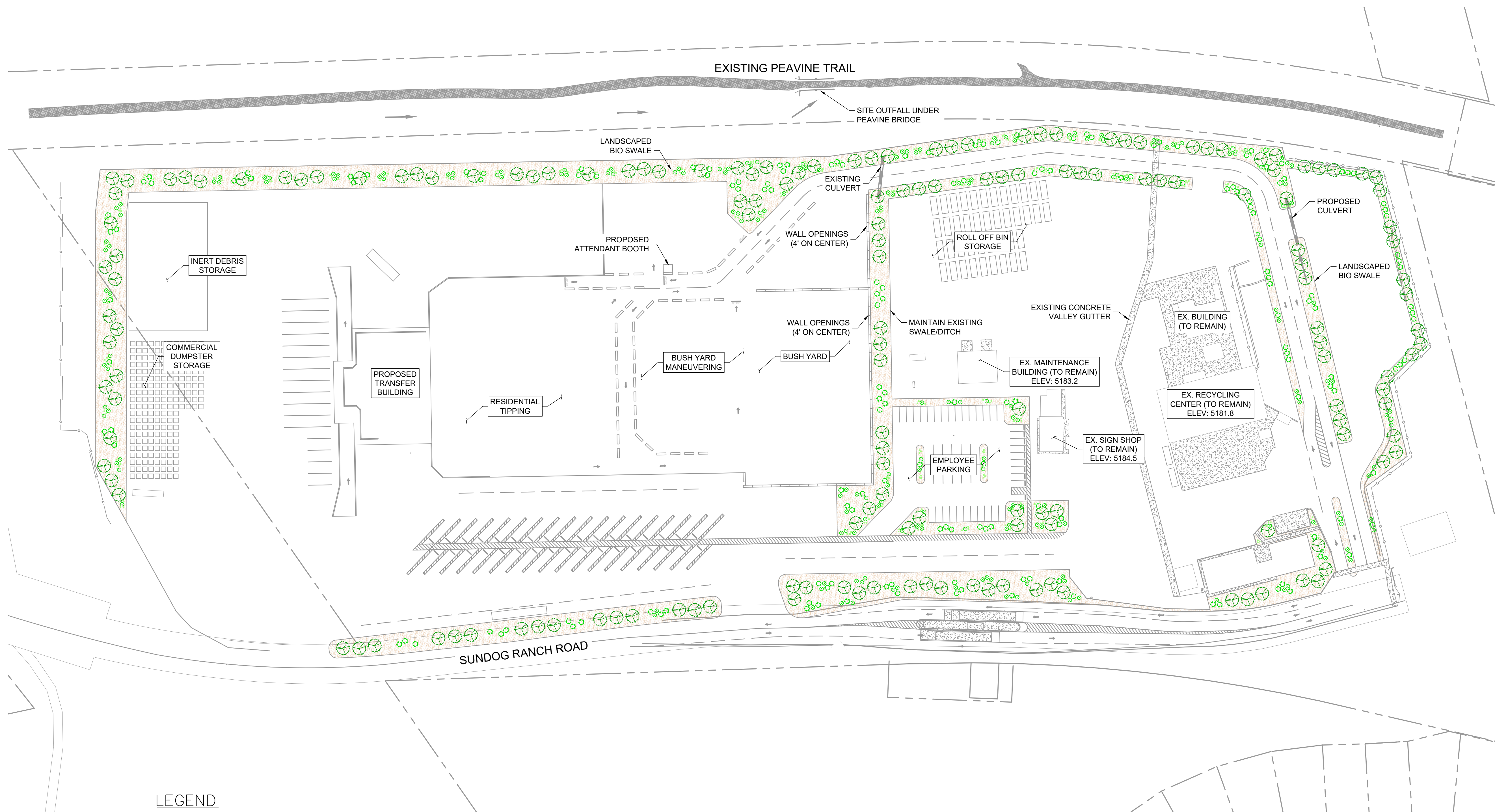
**LEGEND**

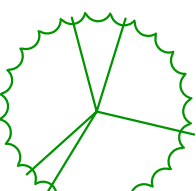



- E— UNDERGROUND ELECTRIC
- T— OVERHEAD TELEPHONE LINES
- FM— SANITARY SEWER FORCE MAIN
- SS— SANITARY SEWER GRAVITY MAIN
- W— WATER LINE
- OHE— OVERHEAD ELECTRIC LINE
- C— TELECOM LINE



CITY OF PRESCOTT PUBLIC WORKS		DWG. NO.	DATE	1/26/2026
<b>PRESCOTT TRANSFER STATION MASTER PLAN</b>		SHEET NO. <b>01</b> OF 01		
<b>CITY OF PRESCOTT</b>		PREPARED BY	CHECKED BY	FOR:
		ALH	ACB	COP
<b>Kimley-Horn</b>		© 2025 KIMLEY-HORN & ASSOCIATES, INC. 101 W. GOODWIN ST., SUITE 303 PHOENIX, AZ 85003 PHONE: 928-448-9700 WWW.KIMLEY-HORN.COM		
		APPRV	DATE	CITY
		REVISIONS		
NO.	BY	DATE	ENGINEER	CIP #:
REVIEW:				

# Appendix N. Landscape Exhibit (Figure 4-1-11)



- LEGEND**
-  LARGE TREE
  -  LARGE SHRUB
  -  SMALL SHRUB
  -  ACCENT

NORTH

GRAPHIC SCALE IN FEET  
0 35 70 140

Call at least two full working days before you begin excavation.  
**ARIZONA 811**  
Arizona 811 Service, Inc.  
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)  
In Maricopa County: (602) 263-1100

<b>CITY OF PRESCOTT</b>		CITY OF PRESCOTT PUBLIC WORKS	DWG. NO.
<b>PRESCOTT TRANSFER STATION MASTER PLAN LANDSCAPE EXHIBIT</b>		DATE 2/12/2026	SHEET NO. <b>01</b>
PREPARED BY:	CHECKED BY:	FOR:	OF 01
<b>Kimley»Horn</b>		CITY	
<small>© 2025 KIMLEY-HORN &amp; ASSOCIATES, INC. 303 101 W. GOODWIN ST., SUITE 303 PHOENIX, AZ 85003 PHONE: 602-446-9700 WWW.KIMLEY-HORN.COM</small>		APPRV. DATE	REVISIONS
NO.	BY	DATE	ENGINEER
			CIP #:



# Transfer Station Master Plan

**JRMA**  
ARCHITECTS ENGINEERS

**Kimley & Horn**

Expect More. Experience Better.

# JRMA Team Introductions

Brett Wolfe (JRMA)

Andrew Baird (KH)

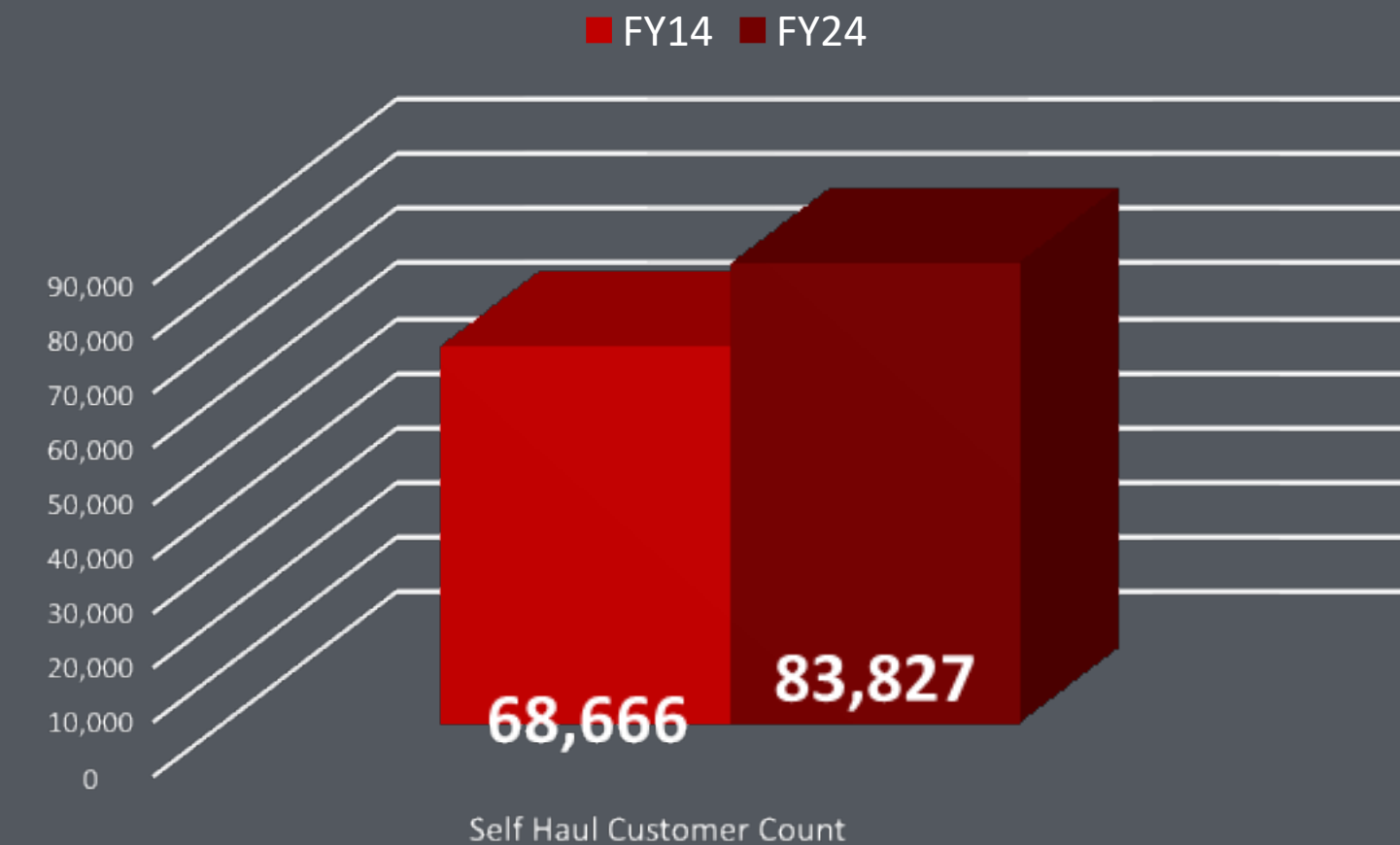
James Yost (JRMA)



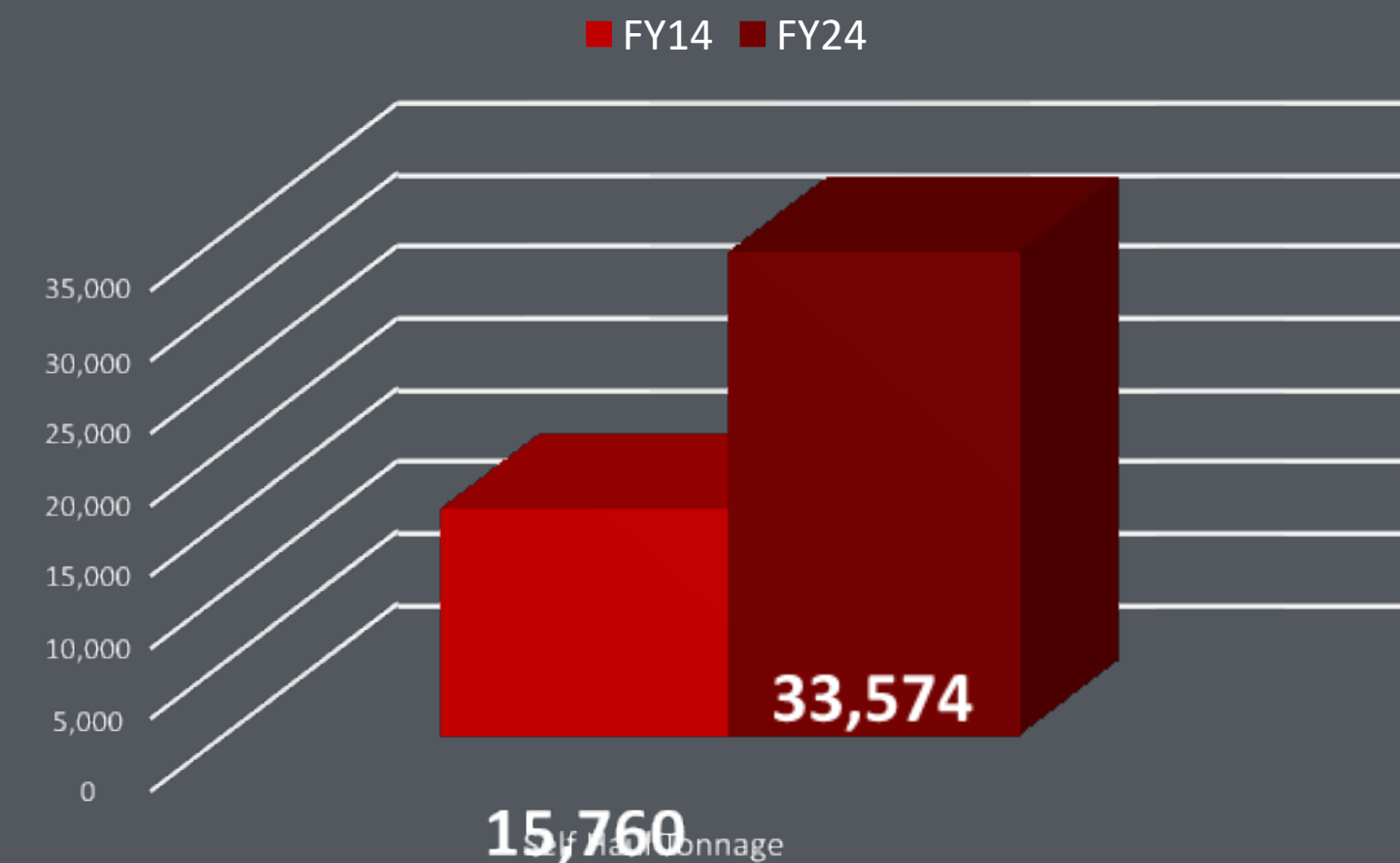
# Background and Reason for the Master Plan Study

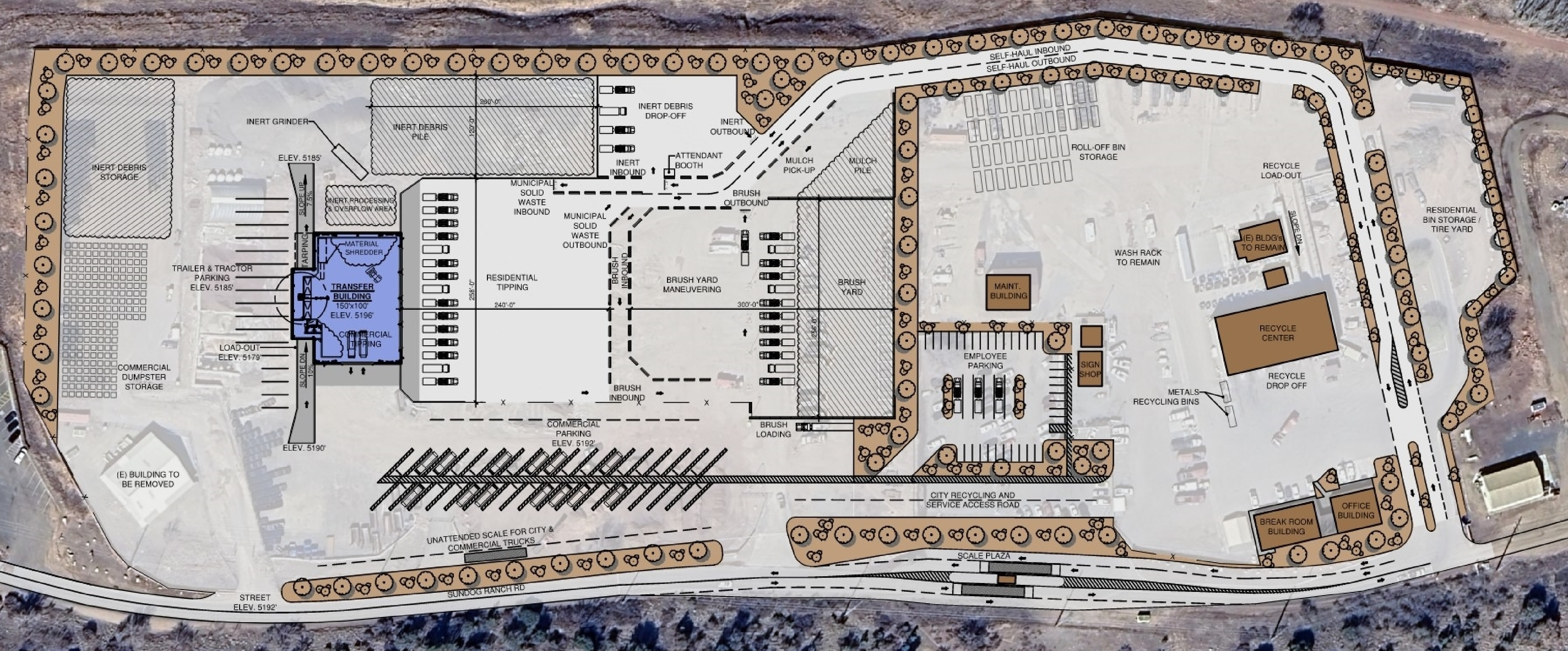
- Significant growth in tonnage and self-haul user trips
- Growth has created capacity issues
- Increased self-haul users presents greater safety risks
- Separation of traffic types (City vs. Self-Haul)
- New layout has more unloading spaces (MSW, Inerts, Green Waste)
- Sundog Ranch Road Transfer Station is a Regional Facility
- From 2014 to 2024 Yavapai County Population grew by 20% (42,000+) to a total of 252,000 people.

### Self Haul Customer Trips Growth



### Self Haul Tonnage Growth





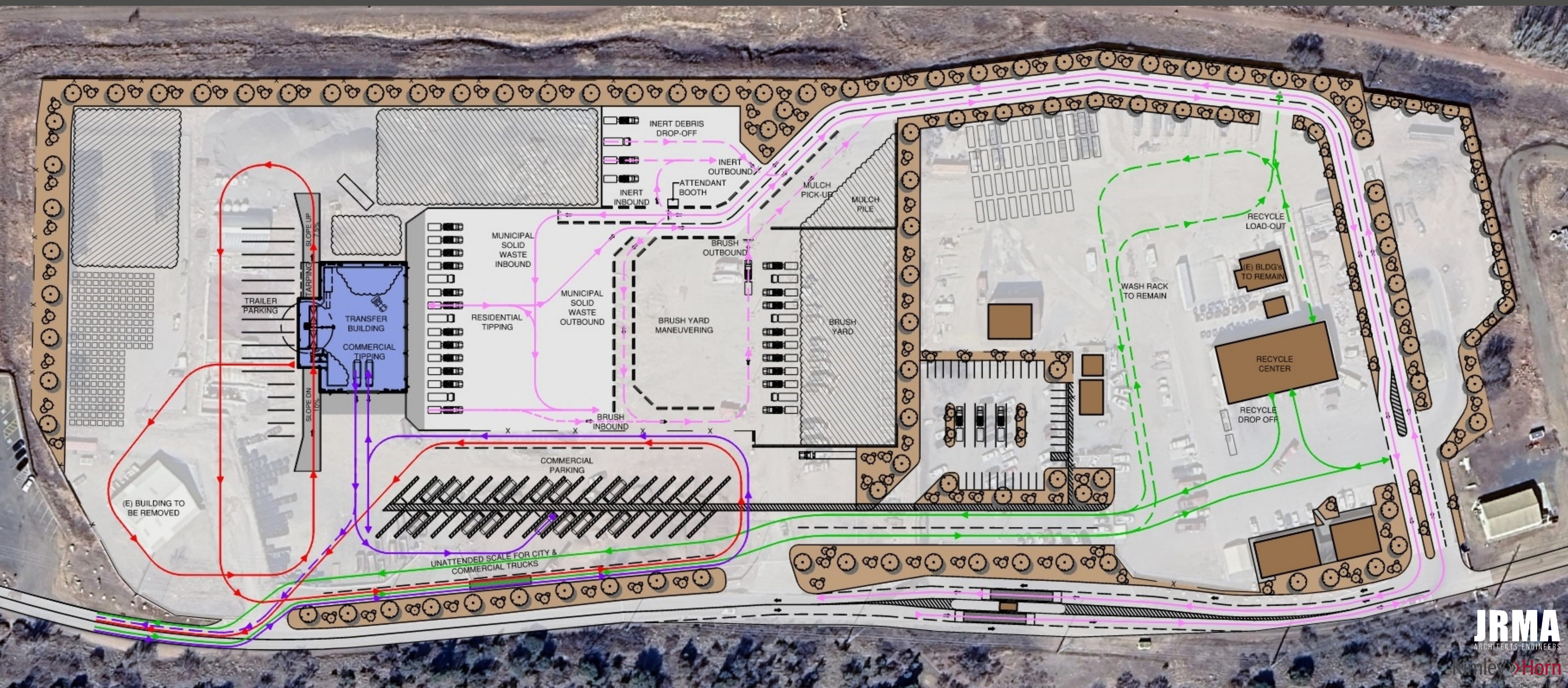
# Transfer Station

## Overall Site

- New 17,000 sq ft Transfer Station and Site Improvements at Existing Sundog Ranch Road Site
- Utilizes New scales and Scale Plaza for Self-Haul
- Leverages Numerous Existing Facilities
- Nearly Complete Separation of Public and City Vehicles

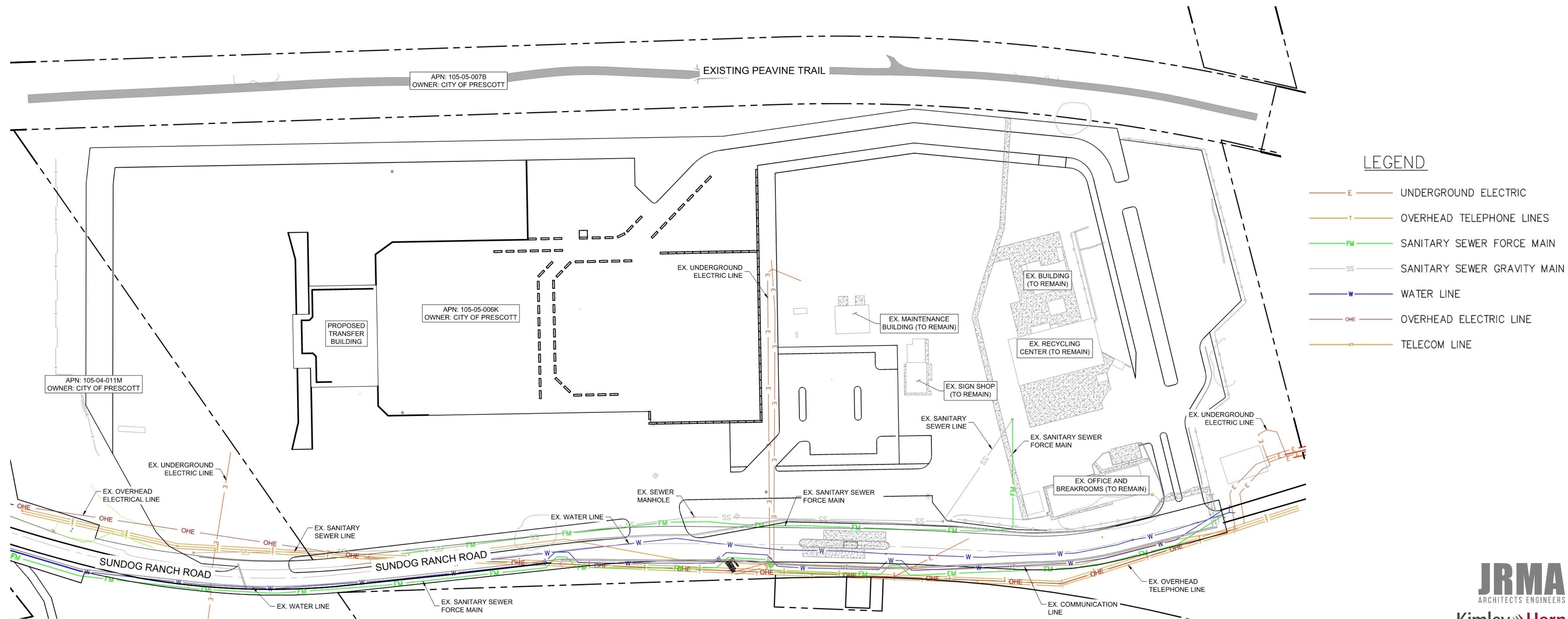
# Site Traffic Flows

Four (4) Traffic Flows – Recycling Collections, Transfer Vehicles, Commercial MSW, and Self-Haul  
 Figure 3-4a to Figure 3-4d (Appendix I-L)



# Site Utility

Figure 4-1-4 (Appendix M)

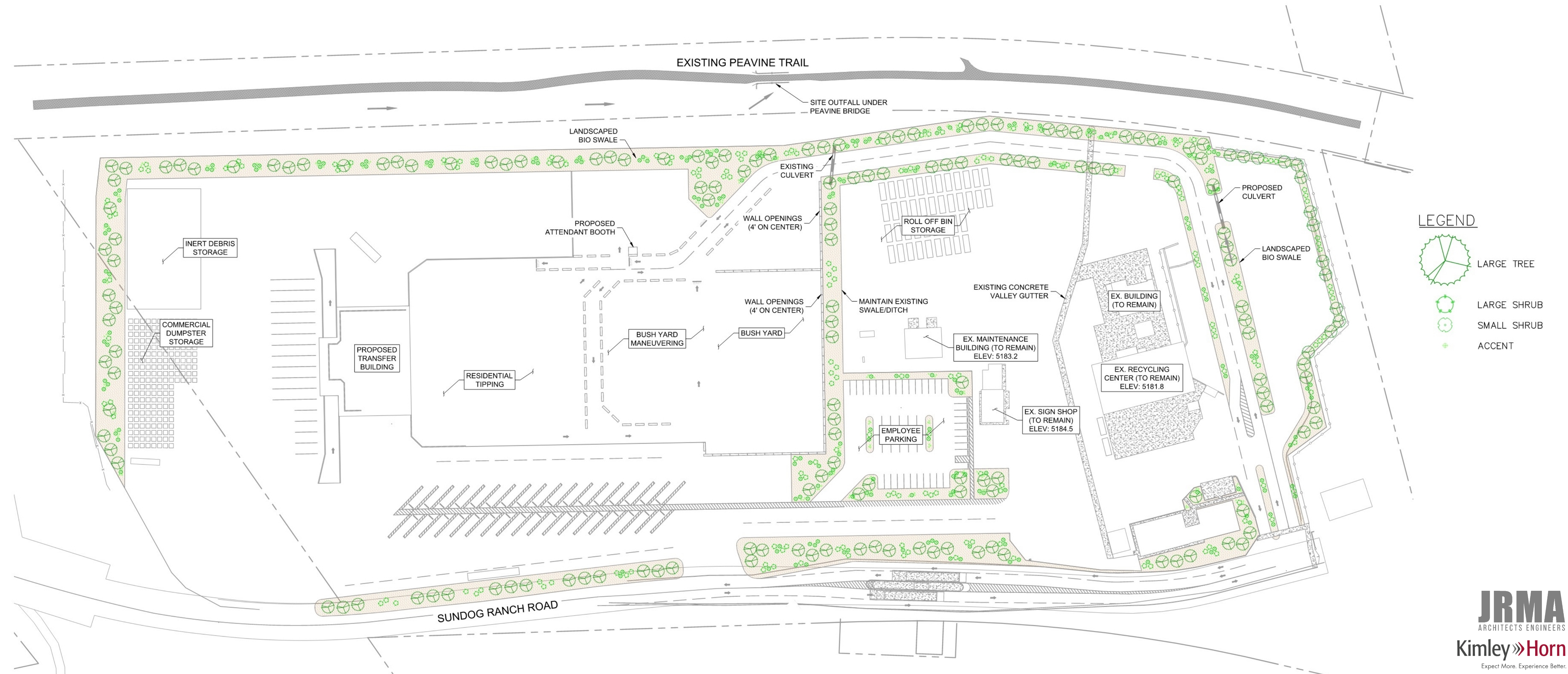


## LEGEND

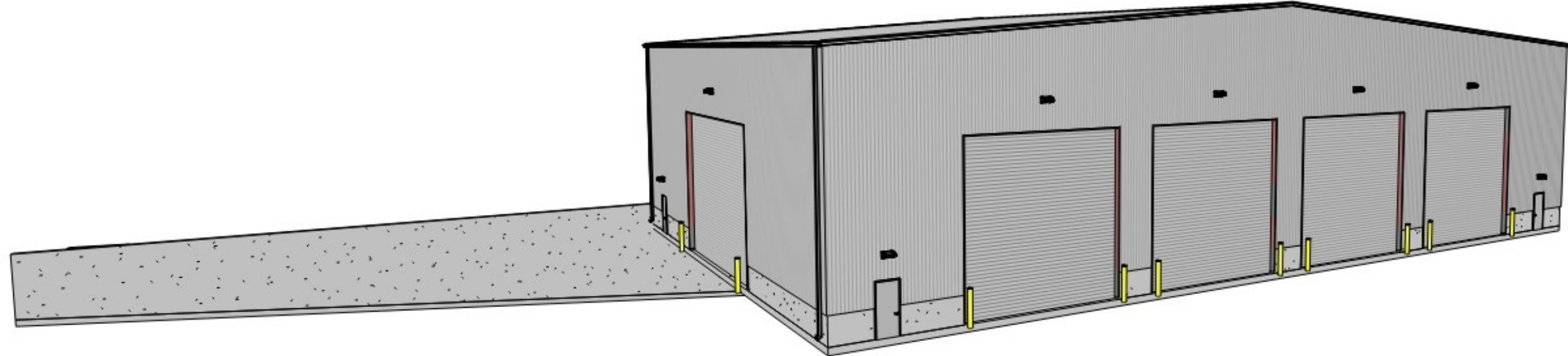
- E** UNDERGROUND ELECTRIC
- T** OVERHEAD TELEPHONE LINES
- FM** SANITARY SEWER FORCE MAIN
- SS** SANITARY SEWER GRAVITY MAIN
- W** WATER LINE
- OHE** OVERHEAD ELECTRIC LINE
- TELECOM** TELECOM LINE

# Landscaping Plan & Environmental

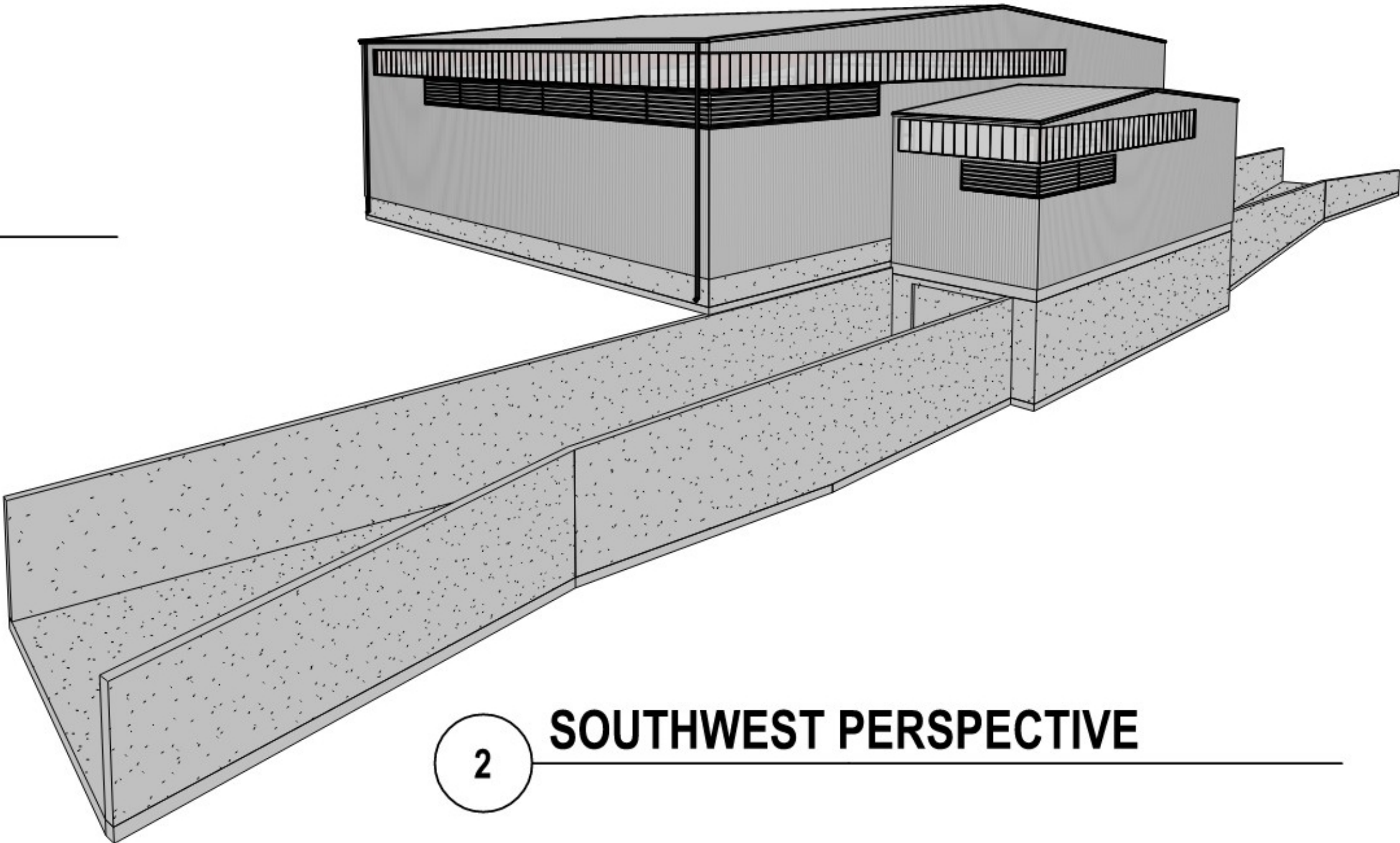
Figure 4-1-11 (Appendix N)



# Transfer Station Exterior Elevations

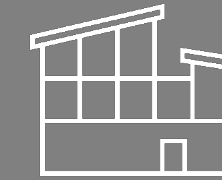
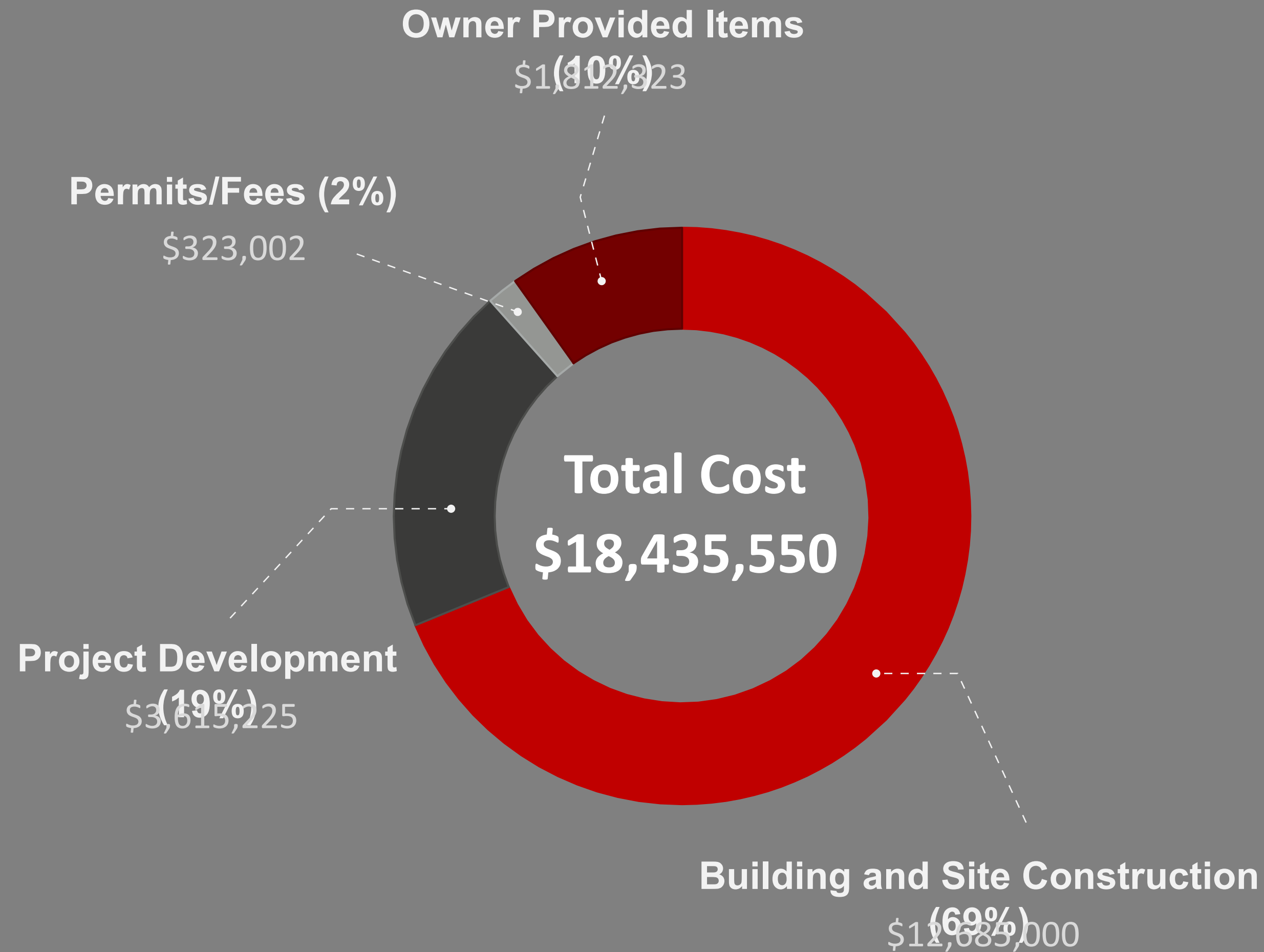


1 **NORTHEAST PERSPECTIVE**



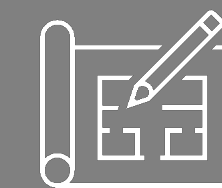
2 **SOUTHWEST PERSPECTIVE**

# Total Project Cost Breakdown



## Building and Site Construction

- Site Construction, Building, and Equipment Costs
- General Conditions for 12 months



## Project Development

- Contractors Management/Admin Costs
- Engineering Costs
- Insurance and Bonding



## Permits/Fees

- Building Permits and Fees
- Utility Connection Fees
- Inspections



## Owner Provided Items

- IT (Data/Security) and Signage
- Furniture, Fixtures, and Equipment

# Evaluation of Relocating Transfer Station



## Primary Impacts of Relocating to a

### New Site

- Estimated cost of a transfer station at new site is \$62M
- NOTE: 4-5% escalation on cost for each year either (existing or new) project is delayed

### Pros:

- Optimal traffic routing and separation of user types
- New site provides all new/updated facilities for City staff
- Existing site will require phased construction

### Cons:

- Existing Buildings/Features would need to be Replicated at New Site (cost)
- Increased environmental and planning efforts for new site
- If new site is not centrally located, potential to see illegal dumping

# Next Steps



## Step 1

Complete Master Plan Narrative with feedback provided today.

## Step 2

Initiate conversations with Yavapai County to discuss existing Intergovernmental Agreement.

## Step 3

Proposed Design for Transfer Station Improvements scheduled for Council Approval in FY31.



# Questions?